

# NOTICE OF REGULAR MEETING

Pursuant to Section 54954.2 of the Government Code of the State of California, a Regular meeting of the Planning Commission is hereby called for:

**Date/Time:** **Wednesday, June 27, 2012, 7:00 p.m.**  
(or as soon thereafter as possible)

**Location:** City Hall Council Chambers  
333 Civic Center Plaza, Tracy

Government Code Section 54954.3 states that every public meeting shall provide an opportunity for the public to address the Planning Commission on any item, before or during consideration of the item, however no action shall be taken on any item not on the agenda.

PLEDGE OF ALLEGIANCE

ROLL CALL

MINUTES APPROVAL

DIRECTOR'S REPORT REGARDING THIS AGENDA

ITEMS FROM THE AUDIENCE

*In accordance with Procedures for Preparation, Posting and Distribution of Agendas and the Conduct of Public Meetings, adopted by Resolution 2008-140 any item not on the agenda brought up by the public at a meeting, shall be automatically referred to staff. If staff is not able to resolve the matter satisfactorily, the member of the public may request a Planning Commission Member to sponsor the item for discussion at a future meeting.*

1. OLD BUSINESS
2. NEW BUSINESS
  - A. **PUBLIC HEARING TO CONSIDER A PROPOSAL TO ADOPT A NORTHEAST INDUSTRIAL SPECIFIC PLAN, AMEND THE TEXT OF THE ZONING ORDINANCE TO INCLUDE A NORTHEAST INDUSTRIAL SPECIFIC PLAN ZONE AND REZONE THE NORTHEAST INDUSTRIAL PLANNING AREA FROM PLANNED UNIT DEVELOPMENT TO NORTHEAST INDUSTRIAL SPECIFIC PLAN. APPLICATION NUMBERS SPA12-0003, ZA12-0006 AND R12-0003**
3. ITEMS FROM THE AUDIENCE
4. DIRECTOR'S REPORT
5. ITEMS FROM THE COMMISSION
6. ADJOURNMENT

**June 22, 2012**

Posted Date

The City of Tracy complies with the Americans with Disabilities Act and makes all reasonable accommodations for the disabled to participate in public meetings. Persons requiring assistance or auxiliary aids in order to participate should call City Hall (209-831-6000), at least 24 hours prior to the meeting.

Any materials distributed to the majority of the Planning Commission regarding any item on this agenda will be made available for public inspection in the Development and Engineering Services Department located at 333 Civic Center Plaza during normal business hours.

## AGENDA ITEM 2-A

### REQUEST

**PUBLIC HEARING TO CONSIDER A PROPOSAL TO ADOPT A NORTHEAST INDUSTRIAL SPECIFIC PLAN, AMEND THE TEXT OF THE ZONING ORDINANCE TO INCLUDE A NORTHEAST INDUSTRIAL SPECIFIC PLAN ZONE AND REZONE THE NORTHEAST INDUSTRIAL PLANNING AREA FROM PLANNED UNIT DEVELOPMENT TO NORTHEAST INDUSTRIAL SPECIFIC PLAN. APPLICATION NUMBERS SPA12-0003, ZA12-0006 AND R12-0003**

### DISCUSSION

#### Existing Concept Development Plan

Annexed to the City in 1996, the Northeast Industrial Area (NEI) is one of the City's major employment areas. The NEI is 870 acres and half built-out (Attachment A, location map). The entire NEI planning area is zoned Planned Unit Development (PUD), which contains development standards that have guided development since its inception (Attachment B, NEI Concept Development Plan).

Prior to construction, all development within a PUD must be approved through a two-step process: (1) a Preliminary Development Plan (PDP), and (2) a Final Development Plan (FDP). Typically bundled for approval, City Council must consider, and if deemed appropriate, approve PDP/FDPs, with consideration of the Planning Commission's recommendation. This means that each time a new project (such as a distribution center, call center, or manufacturing plant) or an amendment to such a project is proposed, the applicant works with staff, then Planning Commission and Council through two public hearings prior to approval. This process, which includes noticing requirements and the time between regularly scheduled hearings, causes certain project approvals to take several months longer than such project approvals would take if a more streamlined process is put in place.

#### Specific Plan Zoning

Because the City has already adopted development standards for NEI through Planning Commission and City Council review, to streamline the process through eliminating two steps of Planning Commission and City Council public hearings, staff received direction from City Council to rezone the NEI project area from PUD to "NEI Specific Plan". This would allow the NEI Specific Plan zoning designation to be written to include a Development Review approval process that the Development Services Director can complete at the staff level. The Development Review process is the current process the City uses for properties that are not zoned PUD. This approval process would still require a public hearing for the benefit of surrounding property owners with a ten-day notice period, but that hearing could be scheduled during normal working hours as opposed to at regular meetings of the Planning Commission and City Council. This would allow for the hearings and overall processes to be streamlined. Under a Specific Plan, any decision to approve a project is appealable to the Planning Commission, and ultimately to the City Council.

### Specific Plan Creation

The NEI Concept Development Plan was written and adopted in 1995 in a format very similar to that of a typical Specific Plan. Staff has re-produced the plan with the necessary edits and formatting (including all previous amendments to the Concept Plan that have already occurred) so it may be adopted as a Specific Plan by Ordinance through a rezoning action. This adoption of the project area as a Specific Plan will not significantly change any of the existing descriptive requirements (such as building design standards, allowable land uses, parking requirements, etc.) as those requirements have proven to be effective in creating consistently successful projects. The Specific Plan will instead serve to streamline the approval process for these projects. The only changes are to building height and allowable uses in the General Commercial land use designation:

- Building Height—Raise the maximum from 46 feet to 60 feet in order to accommodate today's typical interior clear space requirements for warehouse space along with parapet walls for screening roof equipment
- Land Use—Allow for animal shelters to be a permitted use in the General Commercial (GC) land use designation (the NEI contains only three GC sites, and the City's animal shelter is proposed to be located on the GC site at the southwest corner of Grant Line and Paradise Roads).

Tracy Municipal Code Sections 10.20.050 and 10.20.060 of the Specific Plan Ordinance (Attachment C) include the requirements for content of a specific plan and the adoption procedures. All of the necessary content of a specific plan is included in the proposed NEI Specific Plan (Attachment D).

### ENVIRONMENTAL DOCUMENT

The adoption of an NEI Specific Plan, the zone text amendment and the rezone from PUD to NEI Specific Plan are consistent with the NEI Environmental Impact Report (EIR) that was adopted by the City Council in 1996. The project is also consistent with the City's General Plan EIR, adopted by the City Council on February 1, 2011. No new environmental impacts are anticipated as a result of adopting the NEI Concept Development Plan as the NEI Specific Plan, because the proposed Specific Plan does not result in any changes to the proposed land uses, and therefore will not cause any increases in the environmental impacts of those uses.

### RECOMMENDATION

Staff recommends that the Planning Commission recommend that the City Council adopt the NEI Specific Plan, approve a zone text amendment to the Tracy Municipal Code to create an NEI Specific Plan Zone, and approve the rezoning of the NEI project area from PUD to NEI Specific Plan, based on the findings contained in the Planning Commission Resolution dated June 27, 2012 (Attachment E).

MOTION

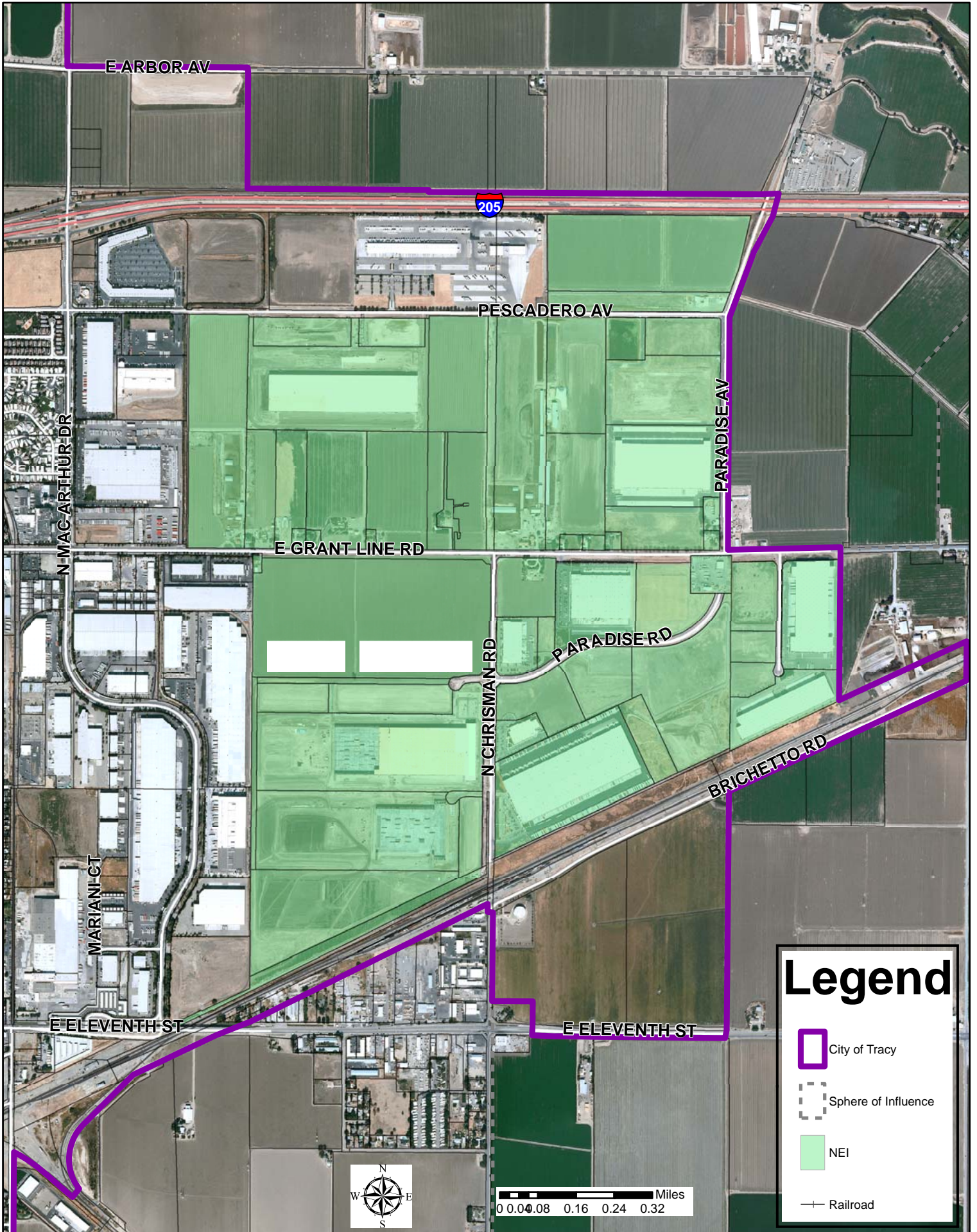
Move that the Planning Commission recommend that the City Council adopt the NEI Specific Plan, approve a zone text amendment to the Tracy Municipal Code to create an NEI Specific Plan Zone, and approve the rezoning of the NEI project area from PUD to NEI Specific Plan, based on the findings contained in the Planning Commission Resolution dated June 27, 2012 (Attachment E).

ATTACHMENTS

Attachment A—Location Map  
Attachment B—Existing NEI Concept Development Plan  
Attachment C—Specific Plan Ordinance  
Attachment D—Proposed NEI Specific Plan  
Attachment E—Planning Commission Resolution

Prepared by: Victoria Lombardo, Senior Planner  
Reviewed by: Bill Dean, Assistant Development Services Director  
Approved by: Andrew Malik, Development Services Director

# Proposed NEI Specific Plan Area



# City of Tracy



Think Inside the Triangle™

# **NORTHEAST INDUSTRIAL AREAS CONCEPT DEVELOPMENT PLAN**

WITH AMENDMENTS:

|          |               |                              |
|----------|---------------|------------------------------|
| 7-98-GPA | Reso 99-106   | Rezoning of Two Parcels      |
| 8-98-GPA | Reso 99-106   | Rezoning of Two Parcels      |
|          | Reso 99-107   | Project Objectives Amendment |
| 45-04-D  | Reso 2005-091 | Chabot Tracy                 |
| 1-08-SPA | Reso 2008-046 | Building Height Amendment    |

**Northeast Industrial  
Concept Development Plan**

November 21, 1995

Revised February 26, 1996



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## INTRODUCTION

The Conceptual Development Plan for the 870 acre Northeast Industrial Area aims to develop a well-planned industrial zone that will attract businesses to Tracy, and provide local employment opportunities. The Plan anticipates a mixture of industrial uses, including rail-dependent industries and "flex-tech" light industrial.

## PLANNING AREA LOCATION

### Location

The Northeast Industrial planning area lies along the northeast boundary of the City of Tracy. The area is generally bounded to the north by I-205, to the south by the Southern Pacific Railroad tracks, to the east by Banta Road, and the west by MacArthur Drive. Grant Line Road bisects the area.

### Surrounding Land Uses

Presently, properties along MacArthur Drive directly west of the site are developed with industrial uses, such as the U.S. Cold Storage facility. North of the site are industrial and commercial uses, including the factory outlet stores at MacArthur Drive and Pescadero Avenue. The Yellow Freight Company is also located to the north, between the site and I-205. Agricultural uses are found on lands to the east.

Lands directly to the north and west of the site are included in the 1991 I-205 Corridor Specific Plan and the 1988 Industrial Area Specific Plan. To the east is the Banta Community Area, as designated in the UMP. The project site in the context of the existing industrial and commercial uses, and existing specific plan areas is shown in Figure 1.

### Land Ownership

The Northeast Industrial planning area is made up of some 32 properties, with a variety of owners. Parcels range in size from small half acre home sites to large agricultural holdings. The assessor's parcel numbers and ownership of the area is shown in Figure 2.

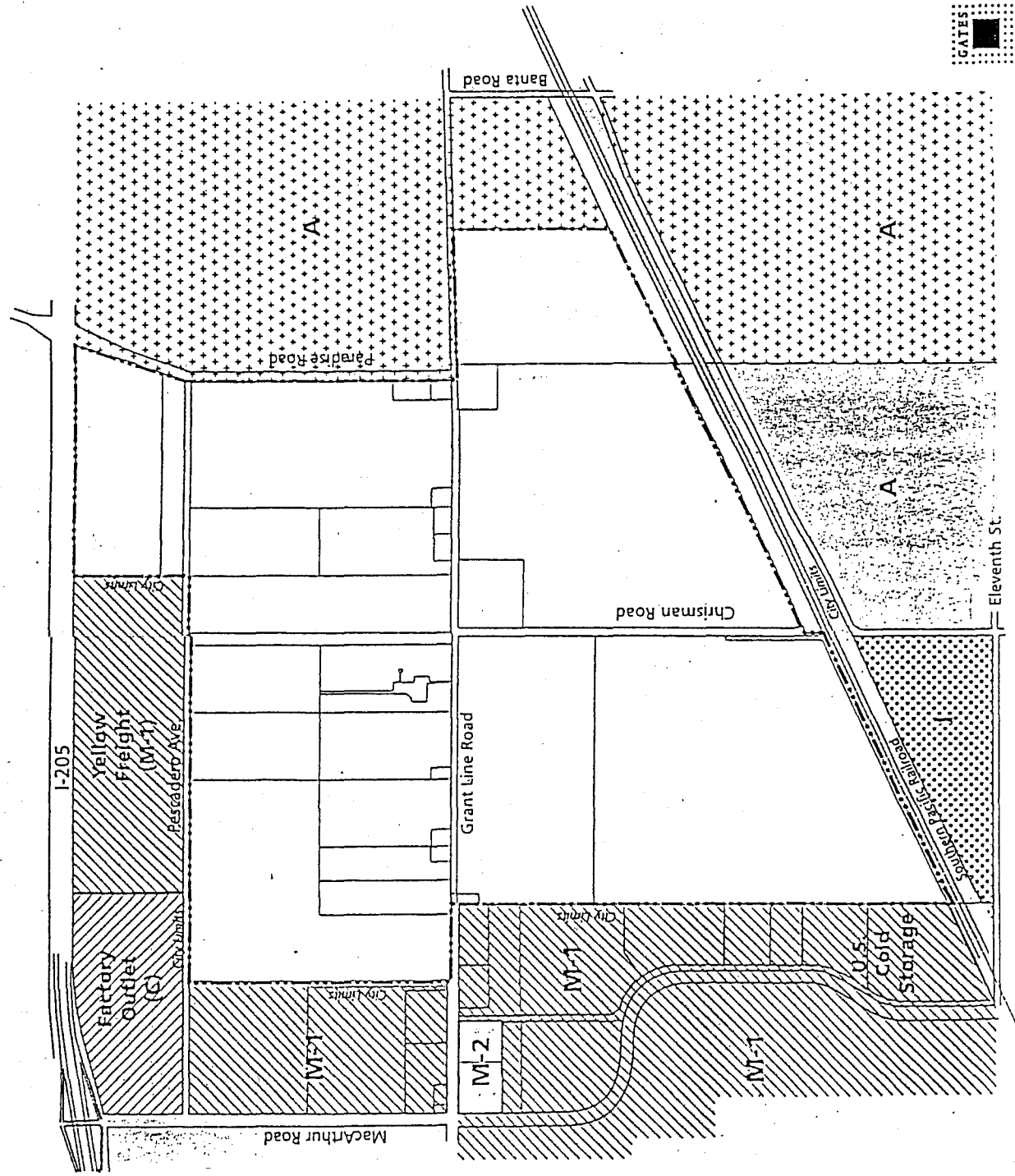
# Northeast Industrial

Figure 1

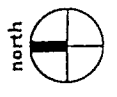
## SURROUNDING LAND USES

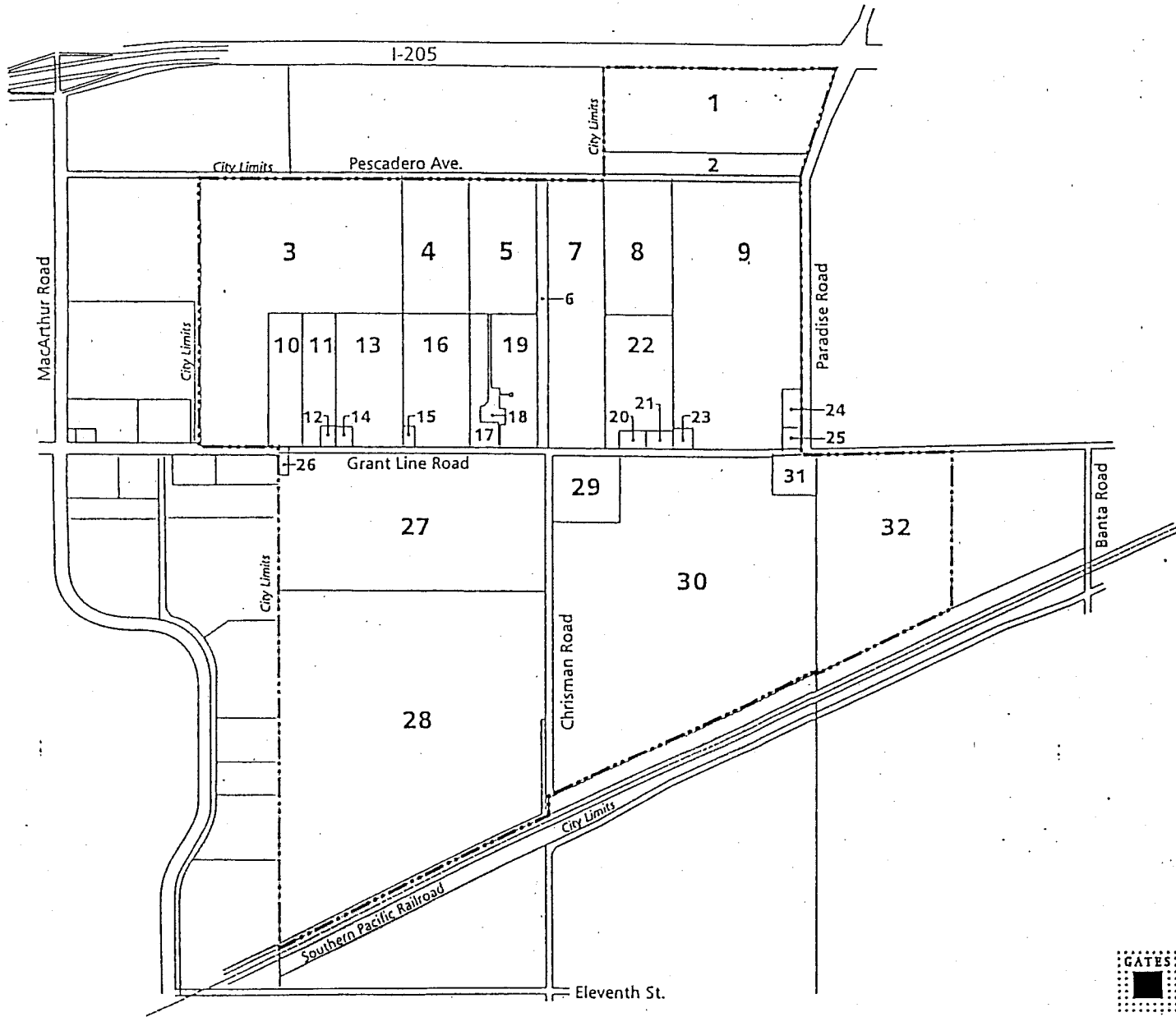
### LEGEND

- Tracy City Limits
- Agricultural Lands (County)
- Industrial Lands (County)
- Industrial Specific Plan
- I-205 Specific Plan
- M-1 Light Industrial Zoning (Tracy)
- M-2 Heavy Industrial Zoning (Tracy)
- Agricultural Lands (Tracy)
- Project Boundary



February 26, 1996





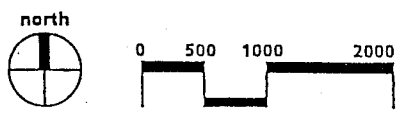
# Northeast Industrial

Figure 2

## LAND OWNERS

| No.            | APN        | OWNER'S NAME             | Acres  |
|----------------|------------|--------------------------|--------|
| 1              | 213-060-12 | SILVA BROTHERS           | 42.47  |
| 2              | 213-060-13 | DOROTHY HALEY            | 9.71   |
| 3              | 213-070-57 | VELMA PRIDENTAL ET AL    | 77.05  |
| 4              | 213-070-05 | VELMA PRIDENTAL AND SONS | 19.24  |
| 5              | 213-070-06 | AUGUST MARTIN            | 19.46  |
| 6              | 213-070-48 | SILVA ET AL              | 5.34   |
| 7              | 213-070-49 | MARIA SILVA ET AL        | 23.46  |
| 8              | 213-070-04 | MARIA SILVA ET AL        | 19.35  |
| 9              | 213-070-43 | FEDRA ASSOCIATES         | 72.04  |
| 10             | 213-070-22 | VELMA PRIDENTAL          | 9.79   |
| 11             | 213-070-28 | HELEN MATTHEWSON         | 9.10   |
| 12             | 213-070-29 | HELEN MATTHEWSON         | 0.68   |
| 13             | 213-070-20 | F. SILVA                 | 14.44  |
| 14             | 213-070-19 | STANLEY ROBERTSON        | 0.72   |
| 15             | 213-070-18 | WEDDELL ENDEA            | 0.52   |
| 16             | 213-070-40 | PRIDENTAL                | 18.79  |
| 17             | 213-070-41 | AUGUST MARTIN            | 6.21   |
| 18             | 213-070-17 | AUGUST MARTIN            | 7.27   |
| 19             | 213-070-39 | AUGUST MARTIN            | 11.41  |
| 20             | 213-070-52 | MARIA SILVA              | 1.04   |
| 21             | 213-070-53 | MANUEL SILVA             | 1.04   |
| 22             | 213-070-51 | MARIA SILVA ET AL        | 17.35  |
| 23             | 213-070-13 | FRANK ENDER              | 0.87   |
| 24             | 213-070-44 | WILLIAM ENDER            | 0.90   |
| 25             | 213-070-45 | MANJORE ENDEA            | 1.62   |
| 26             | 250-020-15 | ROBERT COSTA             | 0.50   |
| 27             | 250-020-14 | DIRA KOCHA ET AL         | 62.50  |
| 28             | 250-020-13 | OLIVERIA FARAS           | 164.10 |
| 29             | 250-020-10 | FRANK SILVA              | 10.00  |
| 30             | 250-020-11 | DOVER ASSOCIATES         | 152.53 |
| 31             | 250-020-02 | FRANCIS BILICOR JR.      | 4.24   |
| 32             | 250-020-01 | TONY COSTA               | 52.00  |
| TOTAL A/P PAGE |            | TOTAL A/P PAGE           | 970.33 |

February 26, 1996



## EXISTING PLANNING AREA CONDITIONS

### Site Features

Land uses within the Northeast Industrial Area are currently agricultural, with a number of dairy operations and rural residences. Few other significant site features are present. The topography is relatively level.

A Westside Irrigation District (WSID) supply or tailwater ditch crosses the area. One property along I-205 is located just outside the FEMA 100 year flood line found in the vicinity of the I-205 / Paradise Road overcrossing. It is believed that theoretical flood depths on the adjacent parcel are only a few feet.

The existing conditions and topography of the planning area are shown on Figures 3 and 4.

None of the parcels appear to be under Williamson Act contracts, although further research should verify the Williamson status.

### Existing Roadway Network

The Plan area is served by a number of existing roadways, some of which have been improved to urban standards and some which remain essentially rural in character.

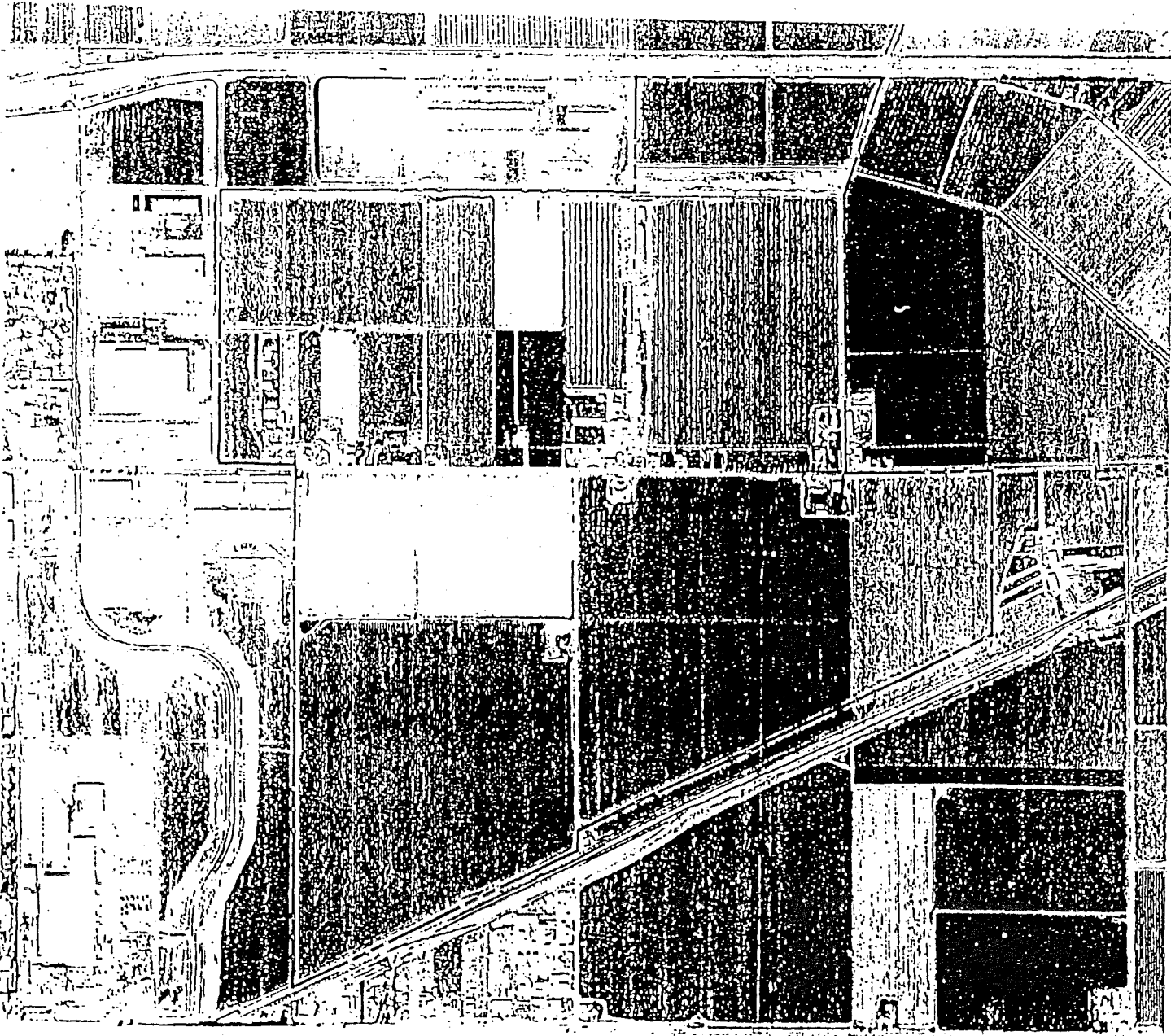
Regional access for the area is provided by Interstate 205 (I-205) to the north and Interstate 5 (I-5) to the east. I-205 is an east-west freeway which extends from I-5 west to I-580 with I-580 continuing west into the Bay Area. I-5 is a north-south freeway which extends throughout Central California. Currently, Plan area access to I-205 is via an interchange at MacArthur Drive. I-5 access is via the full access interchange at Kasson Road (an extension of Grant Line Road). I-5 access is also available (to/from the north only) via an interchange at 11th Street.

The primary local roadways serving the area are MacArthur Drive, Pescadero Avenue and Grant Line Road. MacArthur Drive is a north-south major arterial roadway serving the east side of Tracy. In the Plan area, MacArthur has been improved to a four-lane roadway (with turn lanes) between I-205 and 11th Street. Pescadero Road is an east-west roadway which has been improved to four lanes (with turn lanes) from MacArthur easterly about 1/4 mile. East of this point, Pescadero narrows to a wide two-lane roadway along the Yellow Freight frontage (a distance of about 2/3 mile.) Between this point and Paradise Road, Pescadero is a narrow two-lane rural road. Grant Line Road is an improved four-lane arterial road from MacArthur easterly about 1/4 mile (to the City limits). East of this point Grant Line is a wide two-lane road extending to (and beyond) I-5.

In the Plan area, Chrisman Road and Paradise Road currently provide limited local access. Chrisman is a narrow two-lane rural road which extends from Grant Line Road south to the southern Pacific railroad tracks (Although Chrisman does not cross the tracks). Paradise is a two-lane road extending from Grant Line north over I-205.

### Current Traffic Operations

Current volumes on the area's street network are relatively low. Traffic operations are very stable with area streets operating at Level of Service (LOS) 'C' or better.



# Northeast Industrial


Figure 3

## AERIAL PHOTOGRAPH


source: Pacific Aerial Survey, 10/16/95

February 26, 1996


north



GATES



0 500 1000 2000








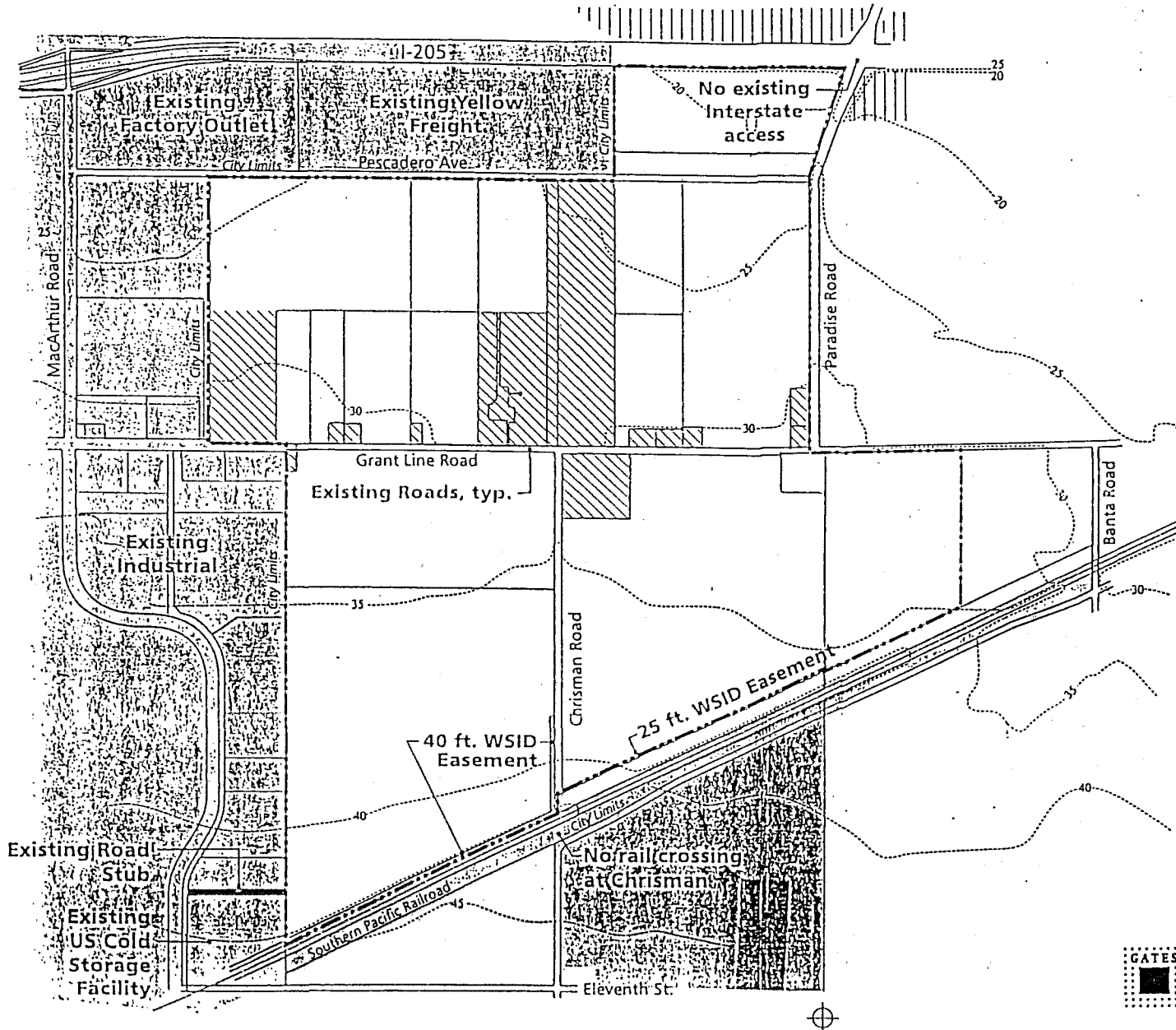
# Northeast Industrial

Figure 4

## EXISTING SITE CONDITIONS

### LEGEND

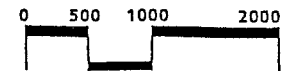
-  FEMA 100 Year Flood Zone
-  Existing Dairies and Homes
-  Tracy City Limits



source: (topography) USGS

February 26, 1996

north



## Existing Utility Infrastructure

### Existing Wastewater Collection Facilities

As shown on Figure 10 (page 21) the East Side Trunk sewer line flows from south to north, and is located west of the subject site and just west of MacArthur Drive. Feeder lines join the East Side Trunk from the west and east.

There is a 12" diameter sanitary sewer on Grant Line Road stubbed to a point about 400' west of the site. There is also a sanitary sewer line on Pescadero Avenue which ranges in size from 18" to 8" diameter. The City of Tracy Public Works Department has indicated this line connects to the East Side Trunk, but has not been able to provide any information about it.

### Existing Storm Drainage Facilities

As shown on Figure 12 (page 25) the most significant existing drainage facility in the vicinity of the Northeast Industrial planning area is the East Side Channel, which flows to the north, west of the site. There is also a 48" diameter storm pipe in Grant Line Road and a pipeline in Pescadero Avenue with a diameter that varies from 15" to 24". These pipes both drain into the East Side Channel.

### Existing Water Distribution Facilities

As shown on Figure 13 (page 27), at this time a 12" to 18" diameter water main exists along Grant Line Road from MacArthur Drive to the west end of the site. A 16" diameter main currently is in place along Pescadero Avenue from MacArthur Drive to the west end of the most northerly portion of the site. There is also a 12" main in MacArthur Drive.

## PLANNING OVERVIEW

This Conceptual Development Plan is submitted as the first step in the planned development process as defined under the appropriate provisions of the Tracy Municipal Code. The Planning steps are envisioned as follows:

### Task 1: Concept Development Plan, Preliminary Development Plan, PD pre-zoning, Finance Plan

The Concept Development Plan will be reviewed by City staff and the City's processing consultant. The City will prepare an EIR, and other documents necessary to complete the CEQA environmental review process. The Concept Development Plan will satisfy both the Concept Development Plan and Preliminary Development Plan requirements of Tracy's Planned Unit Development Ordinance. Once the plan is approved by the City, the area will be pre-zoned PD Planned Unit Development.

A Financing Plan for area-wide public improvements will be submitted separately and prepared in consultation with the City.

### Task 2: Annexation, Development Agreements

Following approval of the Concept Development Plan and receipt of the PD pre-zoning, the lands in the Northeast Industrial Area will be annexed to the City of Tracy and for-

mally zoned PD. Development Agreements may be prepared for each of the properties, at each owners discretion.

### Task 3: Final Development Plans, Subdivision Maps

Each individual land owner will prepare subsequent Final Development Plans (FDP) for their parcels. Subdivision Maps may be submitted with the FDP. Parcelization may occur at any of these steps.

## RELATIONSHIP TO TRACY'S UMP

### UMP Land Use Designations

The City of Tracy Urban Management Plan (UMP) designates the entire area for Industrial land uses, as shown in Figure 5. The UMP designates the planning area as part of the City Core Contiguous area, which encompasses the land areas adjacent to the existing urbanized areas of Tracy. In the City Core Contiguous area, new growth can directly tie into existing service and infrastructure system.

### Tracy's Growth Strategy

In its UMP, the City of Tracy has embraced a "balanced growth" strategy, seeking to direct growth in an efficient cost effective manner, balancing land uses and appropriate use of the land with well planned and utilized infrastructure. This strategy seeks to improve the jobs/housing balance, and to encourage development of employment opportunities and capitalize on freeway interchanges with industrial, retail, and service-related development. An important component of the economic strategy behind this plan is to provide for a more than adequate amount of commercial and industrial land to keep land costs down in order to attract businesses to Tracy, thus providing local jobs for Tracy residents.

The northeastern sector of Tracy has been designated as one area in which the City's industrial growth will occur. Adjacent to existing industrial development, and with direct access to the I-205 freeway and rail transportation, the Northeast Industrial Area will be ideally situated to attract and support business, without the need for major infrastructure expenditures. It is the logical next step for industrial expansion in Tracy.

### UMP Goals Furthered by Northeast Industrial Area Development

The Northeast Industrial Area Concept Development Plan (CDP) proposes development consistent with the UMP. It furthers many of the Goals and Policies stated in the UMP, and it implements many of the Actions set forth in the UMP. Following is a summary of UMP Goals, Policies, and Actions to which the Northeast Industrial Area contributes.

#### Land Use

GOAL LU 1: A balance between residential population, jobs, and ability to provide services.

Action LU 1.1.1: Industry will be generally targeted to the northeast, south and southwest of Tracy.

Policy LU 2.4: Provide flexibility in land use planning (including regulation of land use intensity, distribution, balance) to be responsive to market.

GOAL LU 6: A land use mix that provides employment opportunities for all who live in Tracy and wish to work here.

Action LU 6.1.2: Designate area around existing and planned industry for compatible land uses.

Policy LU 6.2: Encourage a diversity of industries.

Policy LU 6.3: Maintain Tracy's competitiveness in attracting industries looking to relocate to the Central Valley.

GOAL LU 7: Land use patterns that minimize conflicts between neighboring uses and transportation corridors.

Policy LU 7.3: Locate compatible development near and along freeway corridors, and provide adequate environmental protection to less compatible uses.

Policy LU 7.4: Implement freeway uses which are compatible with the noise, air quality, traffic impacts associated with freeways.

Action LU 7.5.1: Locate employment-generating and regional commercial uses along major transportation corridors.

Policy LU 7.7: Establish land uses along freight rail lines that are consistent and compatible with rail service and exposure.

GOAL LU 8: Continue agriculture and resource extraction for as long as they can be conducted in an economically viable fashion.

GOAL LU 9: Maintain economic viability as a community.

Policy LU 9.2: Tracy is to remain a central urban service provider and become a retail and industrial center.

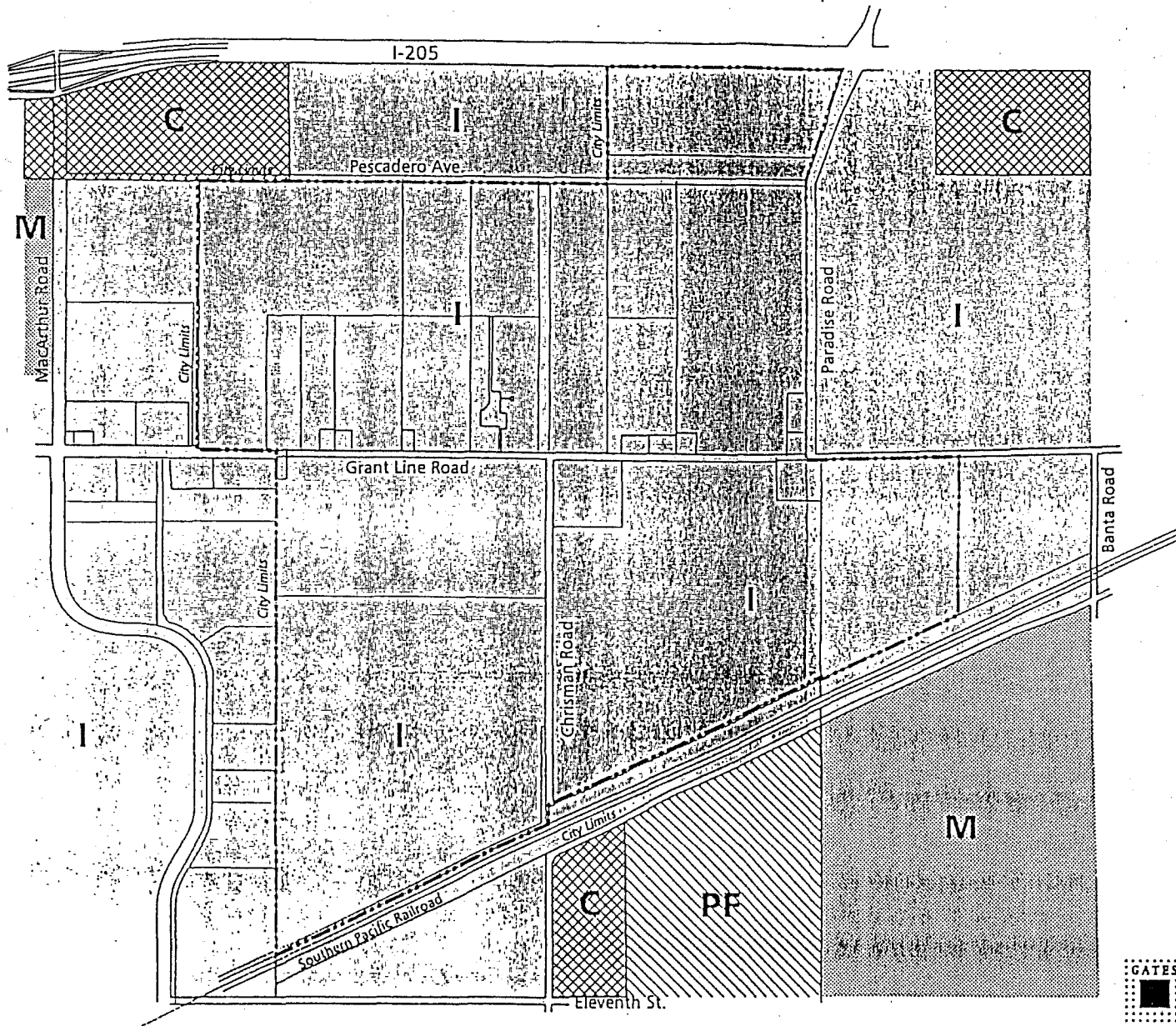
### Circulation

GOAL CI 3: Minimize use of city streets by inter-regional commuter traffic and trucks.

Policy CI 3.4: Achieve direct movement of trucks from major generators to the freeways and other inter-regional routes via streets planned and designed for heavy vehicles.

Action CI 3.4.4: Limit new truck traffic generating uses to locations along the freeways, expressways and truck routes.

GOAL CI 8: Efficient movement of truck traffic through and around the city with minimum impacts on residential and commercial areas.







# Northeast Industrial

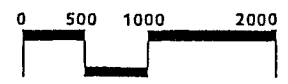
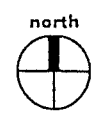
Figure 5

## UMP LAND USES

### LEGEND

-  Commercial
-  Industrial
-  Public Facilities
-  Medium Density Residential

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## PROJECT CHARACTERISTICS

### Project Objectives

The objectives of the project are:

- To develop the Northeast Industrial Area as a high-quality industrial and commercial site of significant benefit to the City of Tracy and the nearby region.
- To develop a well-planned site that will attract businesses to Tracy, providing local employment opportunities.
- To develop the Northeast Industrial Area for primarily mixed industrial uses including rail-dependent industries.
- To minimize project-related impacts to Tracy's transportation network.
- To provide a flexible phasing program that allows market forces to dictate reasonable growth increments, while ensuring that agricultural properties are allowed to remain until ready to develop.
- To create a project consistent with the goals of the UMP.
- To integrate the Northeast Industrial Area into the development pattern of the City of Tracy.
- To integrate mitigation for environmental impacts into the design of the project.

### Land Use Designations

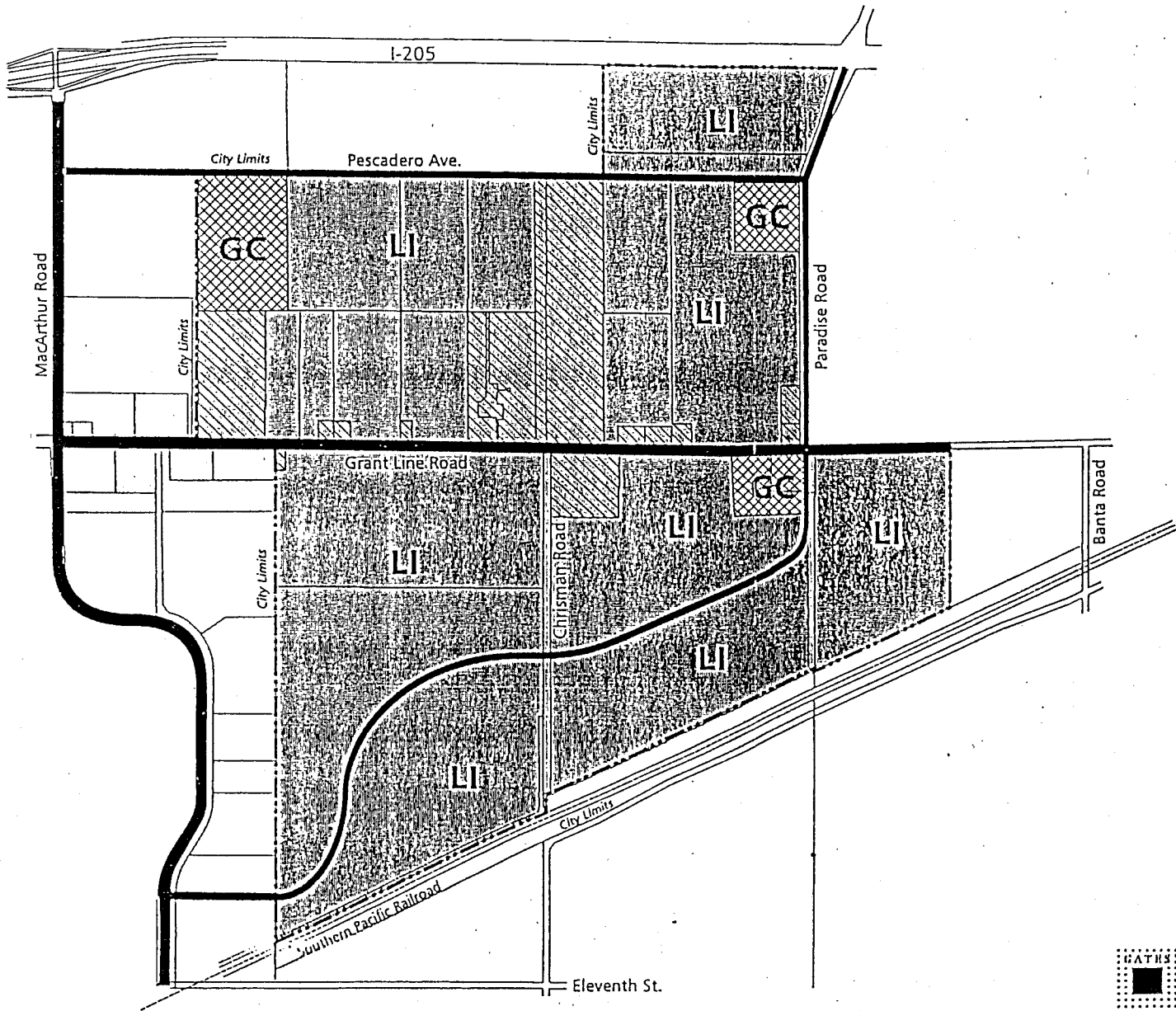
The Northeast Industrial Area will consist primarily of light industrial land uses. In addition, general commercial land uses are planned at major intersections along Pescadero Avenue and Grant Line Road. Figure 6 shows the configuration of the land uses in the planning area.

The development prototypes included in the Design Guidelines indicate possible site planning scenarios for environmental impact analysis.

#### Light Industrial Land Uses (LI)

Light Industrial land uses cover approximately 800 acres of the planning area. Assuming a Floor Area Ratio (FAR) of 0.5, approximately 17.5 million square feet of light industrial, warehouse and manufacturing facilities would be provided. Light industrial land uses would be compatible with existing industrial land uses to the west and north, as well as with freeway noise, and rail noise and vibration.




Several types of light industrial land uses are appropriate in the Northeast Industrial Area. It is anticipated that warehousing and distribution businesses with low employee densities will be the predominant development type. This development pattern is similar to those that have located in Tracy in recent years. The southern portion of the Planning Area are appropriate to uses that require rail access. It is assumed that about 80% of the light industrial lands will be developed with warehouse and distribution similar to the type of development existing in Tracy.



# Northeast Industrial

Figure 6  
**LAND USE PLAN**

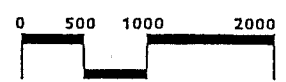
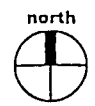
**LEGEND**

-  General Commercial
-  Light Industrial
-  Existing Dairies and Homes

**DEVELOPMENT SUMMARY**

| Land Use                | Gross acres  | FAR  | Square feet       |
|-------------------------|--------------|------|-------------------|
| General Commercial (GC) | 45.5         | 0.35 | 694,000           |
| Light Industrial (LI)   | 798.9        | 0.50 | 17,400,000        |
| Proposed Roads          | 15.8         | --   |                   |
| Existing Road Expansion | 10.1         | --   |                   |
| <b>TOTAL:</b>           | <b>870.3</b> |      | <b>18,094,000</b> |

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The City of Tracy is also interested in attracting higher employee density businesses to the area. It is anticipated that there may be a future demand for a "Flex-Tech" development that would accommodate research & development businesses. For purposes of analysis, it is assumed that about 20% of the light industrial lands will be developed with such higher employee density businesses.

The light industrial zone may also be appropriate for service commercial businesses with little pedestrian traffic, that are not necessarily compatible with general commercial land uses, such as automotive supply and plumbing stores.

A variety of parcel sizes are foreseeable, probably ranging from 20 to 50 acres, depending upon the type of industrial user attracted to the area. Since market forces will dictate that eventual parcel size, the CDP does not reflect an exact parcelization plan.

#### **Existing Agricultural and Residential Uses**

The parcels containing existing dairy operations and residences, shown as crosshatched on the Land Use Map, that may remain for a number of years until ready to develop. The planned light industrial land uses will be compatible with these properties. (The agricultural lands are subject to the City's Right to Farm Ordinance, discussed below)

#### **General Commercial Land Uses (GC)**

Three parcels of general commercial, totaling 45.5 acres, are provided within the planning area. These commercial sites are intended to expand upon the existing commercial uses adjacent to the I-205/ MacArthur Drive interchange, take advantage of the freeway-oriented traffic circulation of the site, and serve the local industrial community.

The largest parcel (25.5 acres) is located in the northwest corner of the project along Pescadero Avenue. This parcel provides for potential factory outlet expansion or other interstate-related commercial uses. Two ten acre parcels are located at the Grant Line/ Paradise intersection and the Pescadero / Paradise interchange. These two smaller parcels are intended to provide commercial and retail services to the planned industrial community.

#### **Right to Farm Provisions**

A number of dairy operations currently exist within the Northeast Industrial planning area. Dairy facilities typically involve significant capital improvements. This CDP intends to allow the continued operation of these existing dairy facilities and other agricultural uses until such time as the owners wish to convert their property to nonagricultural uses.

When nonagricultural land uses move into areas near to pre-existing agricultural operations, the agricultural operations frequently become the subject of nuisance complaints. Chapter 7 of the Tracy Municipal Code, the Right to Farm Ordinance, declares that farming operations are not a nuisance, and recognizes the right to farm within the incorporated City. Agricultural and dairy operations are included in the Permitted uses (page 43) for both the Light Industrial and General Commercial land use designations.

#### **Mineral Extraction**

A number of the parcels in the Northeast Industrial planning area have mineral rights that are owned by different owners than the underlying land. It is possible that these owners may choose to



exercise these rights at some time in the future. As such, mineral extraction is included as a Conditionally Permitted use within the Light Industrial land use designation.

Any mineral extraction operation will be unobtrusive, and will be a subsidiary use to the primary use of the parcel.

## CIRCULATION AND TRANSPORTATION

The Northeast Industrial Concept Development Plan provides for efficient circulation by automobiles and trucks. The proposed land-use mix, street geometry and proximity to the interstate freeway system will minimize project-related impacts to Tracy's transportation network.

### Background Traffic Growth / Traffic Operations

The City has prepared projections of traffic flows for baseline development. This baseline development includes the Industrial Specific Plan (ISP).

The future background traffic projections have been derived from the Tracy citywide travel demand model. The model's basic assumptions related to trip generation rates and traffic distribution were used in developing the baseline traffic projections. It is noted that model projections have been based on a conservatively high assumption that industrial development has an employee density of 11 employees per acre.

With respect to traffic operations, the ISP improvements and site specific improvements on MacArthur Drive, Pescadero Road, and Grant Line Road have been completed. These improvements allow for stable traffic flow conditions (LOS C or better) with buildout of the ISP development.

### Traffic Increases Due to the Project

#### Traffic Generation

The plan includes primarily industrial development with ancillary (local serving) commercial and a larger commercial retail center which would potentially attract customers from outside the Plan area. The industrial areas are envisioned as primarily warehouse and distribution developments with a smaller amount of more intense light industrial / flex tech development. The following assumptions are provided for the purposes of the Plan's traffic analysis :

- 123 acres of warehouse and distribution development with 8 employees per acre
- 676 acres of light industrial development with an average of 11 employees per acre. (Assumes that 507 acres (75%) are warehouse and distribution uses with 8 employees per acre, and 169 acres (25%) are flex-tech uses at 20-30 employees per acre, resulting in the average of 11.)
- 20 acres of small commercial areas designed to serve the internal needs of the Plan area employees
- 25.5 acres of outlet mall type retail development serving subregional customers.

## Circulation Plan

### Proposed Internal Street Network

As shown in Figure 7, the Plan's internal street network would include existing roadways as well as a new loop street. The street descriptions are as follows:

- Pescadero Avenue would be a two-lane "modified industrial street", with a 76 foot right of way, extending from the terminus of the four-lane section easterly through the Plan area to Paradise Road.
- Grant Line Road would be a four-lane "major arterial street", with a 110 foot right of way, from the terminus of the current four-lane section easterly through the entire Plan area.
- Paradise Road would be a two-lane "Phase I arterial street" extending from the I-205 overcrossing south to Chrisman Road. Beyond Chrisman Road the street would continue to the south and west with a two lane "Industrial Street" connection to MacArthur Road.
- Chrisman Road would be developed as a two-lane "Industrial Street" north of the loop street, to allow convenient parcel access during buildout. South of the loop street, the existing roadway section would be retained in the current condition, with no improvement.
- Additional two-lane "Industrial streets" may ultimately be provided in the Plan area, located in response to specific parcelization and development proposals in the area. Spacing between industrial street intersections would be at least 660 feet.

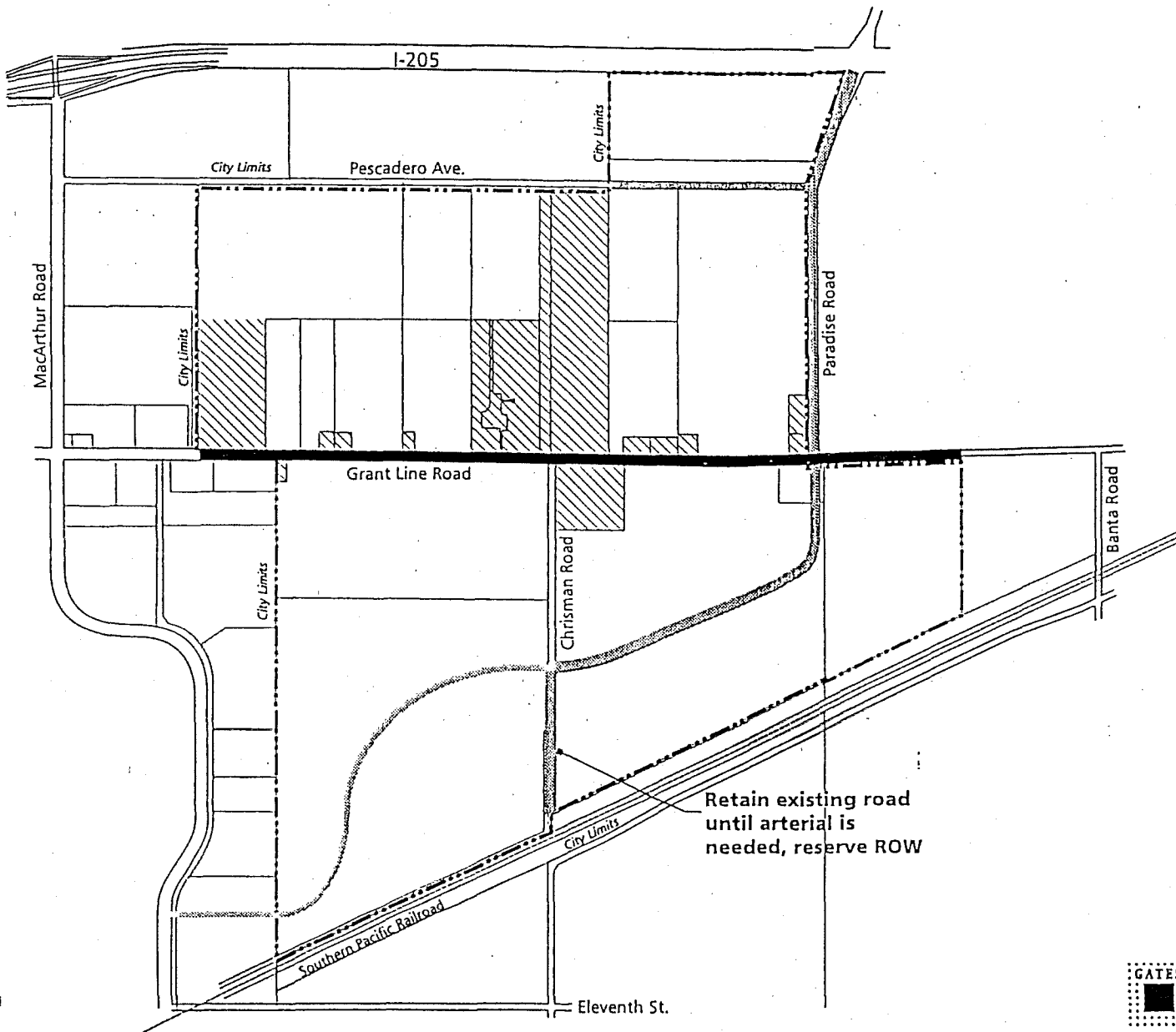
Figure 8a & 8b indicates the proposed cross sections of the internal streets.

### Parcel Access

Because the Plan does not include specific development parcel details (these will occur as the Plan is implemented), it would be tenuous to identify specific site access. However, the City has studied access issues (Conceptual Design Standards for the Tracy Roadway Master Plan, Fehr & Peers Associates, September 22, 1994). Within the context of these recommended standards, access on the major Plan area roadways (Grant Line Road and Paradise Road-Loop Street) should adhere to the driveway spacing guidelines found in the following Design Guidelines.

### Rail Access

A number of parcels front the Southern Pacific Railroad right-of-way to the south of the planning area. Railroad spurs accessing the rail line may be provided for rail-dependent industries that located in this part of the Planning Area.








# Northeast Industrial

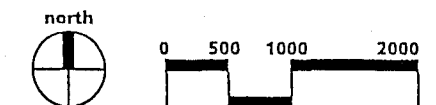
Figure 7

## PROPOSED ROAD NETWORK

### LEGEND

-  4 Lane Arterial, 110' ROW
-  Phase I Arterial Street, 110' ROW
-  Modified Industrial Street, 76' ROW
-  Industrial Street, 60' ROW
-  Existing Dairies and Homes

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### Truck Routes

Grant Line Road and MacArthur Drive are currently designated as truck routes in the UMP. While trucks may use other streets inside the area, such use would only be as needed to access a specific destination. Truck traffic towards destinations outside of the Northeast Industrial area will be directed towards the designated truck routes.

### Relationship Between the Proposed Circulation Plan and the Tracy Roadway Master Plan

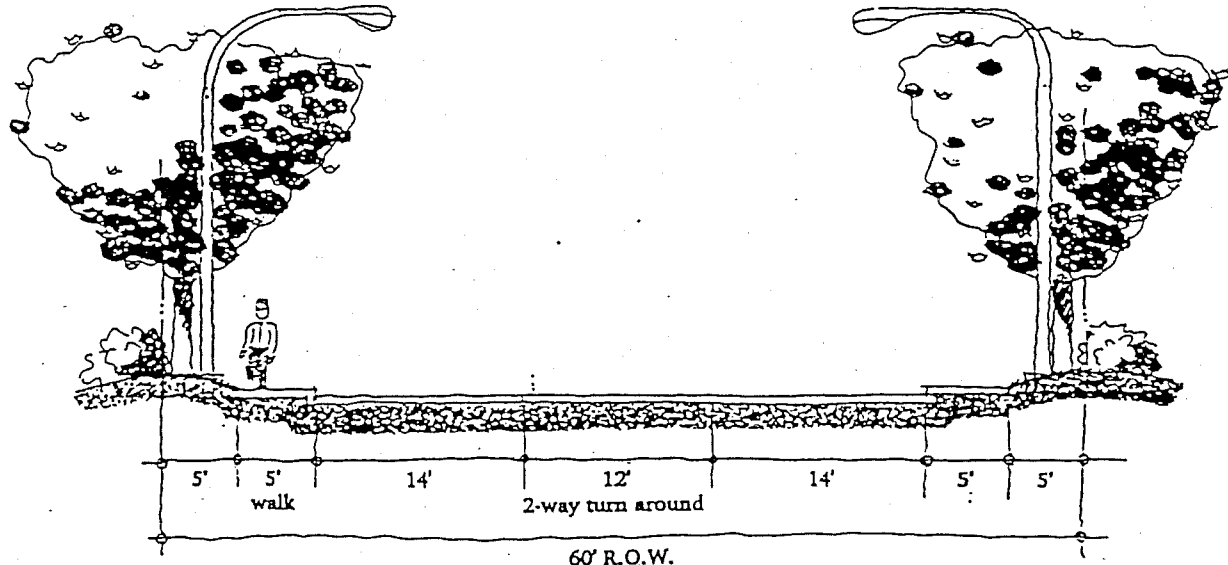
The City of Tracy Roadway Master Plan (RMP) was prepared in response to long-term buildout of the City's Urban Management Plan (UMP). As such, this roadway system was intended to serve 35 years of development growth, with an ultimate system (involving right-of-way preservation) that would probably not be needed for 40-50 years.

The two major RMP corridors through the Plan area are Grant Line Road (east-west) and Chrisman Road (north-south). The RMP envisioned that with ultimate long-term buildout, these roadways would carry heavy through volumes due to extensive development in the east areas of Tracy. The Grant Line expressway was intended to carry heavy through volumes to and from I-5, east of the City. The Chrisman expressway was intended to provide an alternative to MacArthur Drive, carrying heavy through volumes to and from I-205 (with a new I-205 interchange).

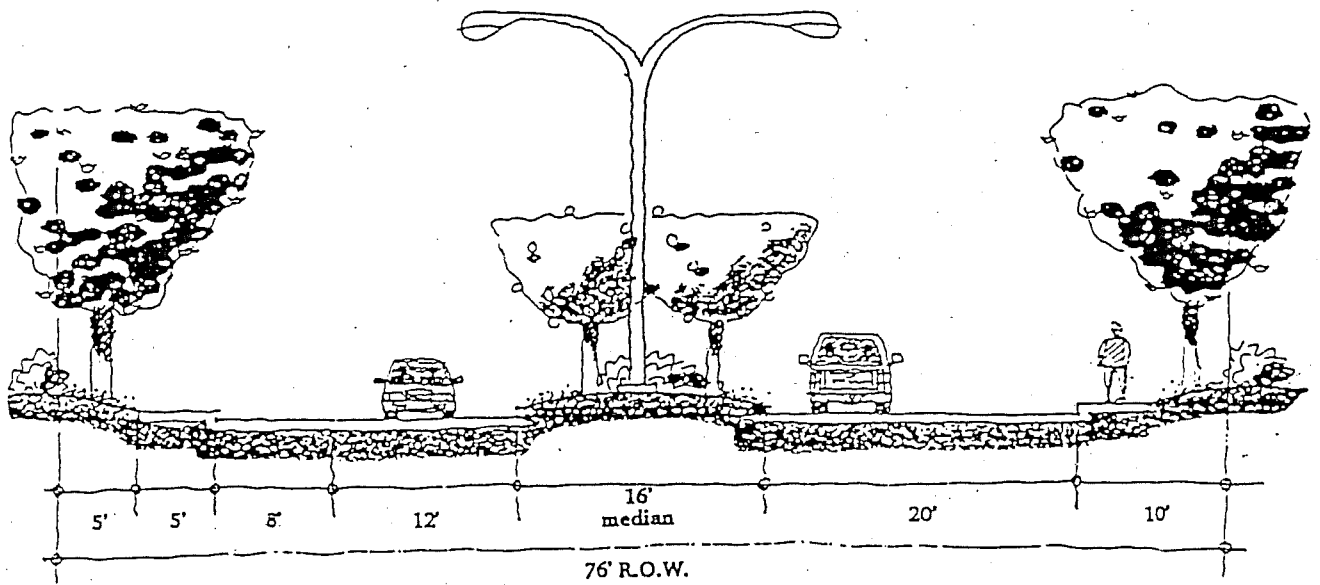
Because the RMP ultimate system is based upon extremely long-term growth projections, it is not feasible for the proposed Circulation Plan to precisely coincide with the ultimate system. The Circulation Plan differs from the ultimate RMP for two distinct reasons. First, the RMP envisions major six-lane expressways along Chrisman Road and Grant Line Road through the Plan area. Reserving the extremely wide rights-of-way for these roadways would bisect the Plan area (in both directions) and significantly impact the flexibility of future parcelization and development. More importantly, the expressways would create substantial development frontages with extremely limited access (as per the RMP recommendations). This severely restricted access would further impact the ability to market and develop parcels within the Plan area.

Nonetheless, the proposed Circulation Plan does recognize the need to provide the potential for future expansion to wider roadways. As shown in Figure 7, the Circulation Plan would exactly follow the Grant Line Road alignment, providing a four-lane major arterial street through the Plan Area. Within the arterial street 110 foot right-of-way, the cross-section could be modified (through a narrower median and elimination of shoulder areas) to accommodate the potential need for six-lanes.

A number of viable dairy facilities are in the path of a direct Chrisman Road access to I-205. Due to the disruptive impacts of such an alignment, the Circulation Plan proposes to allow for an alternative north-south roadway in the Chrisman Road - Paradise Road corridor. Although the Circulation Plan only indicates a two-lane modified industrial street, the Plan does provide for a right-of-way width that would accommodate a potential four-lane major arterial. Again, modifications to the four-lane arterial could allow for six lanes if such capacity is ever needed. This alignment also allows for an ultimate I-205 interchange at the existing Paradise Road overcrossing of I-205. It is noted that if an interchange is ultimately located at Paradise Road, it would be about 1.5 miles east of the MacArthur interchange and this spacing would satisfy State and Federal design standards.



Industrial Street

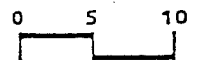


Modified Industrial Street Section

Figure 8A

**STREET CROSS SECTIONS**

**Northeast Industrial**

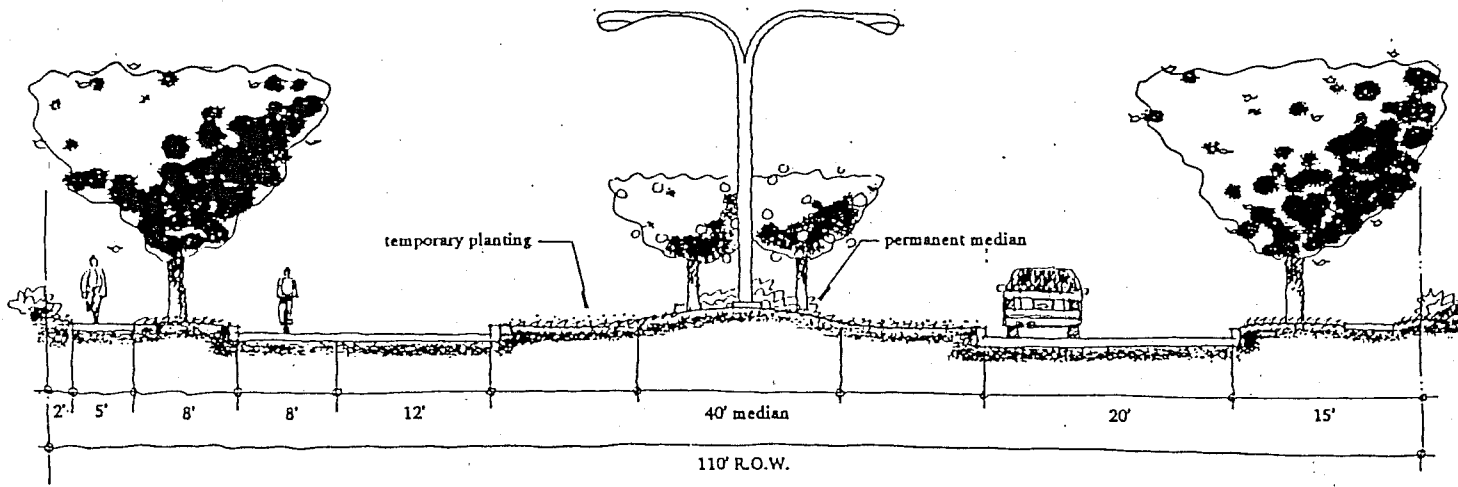


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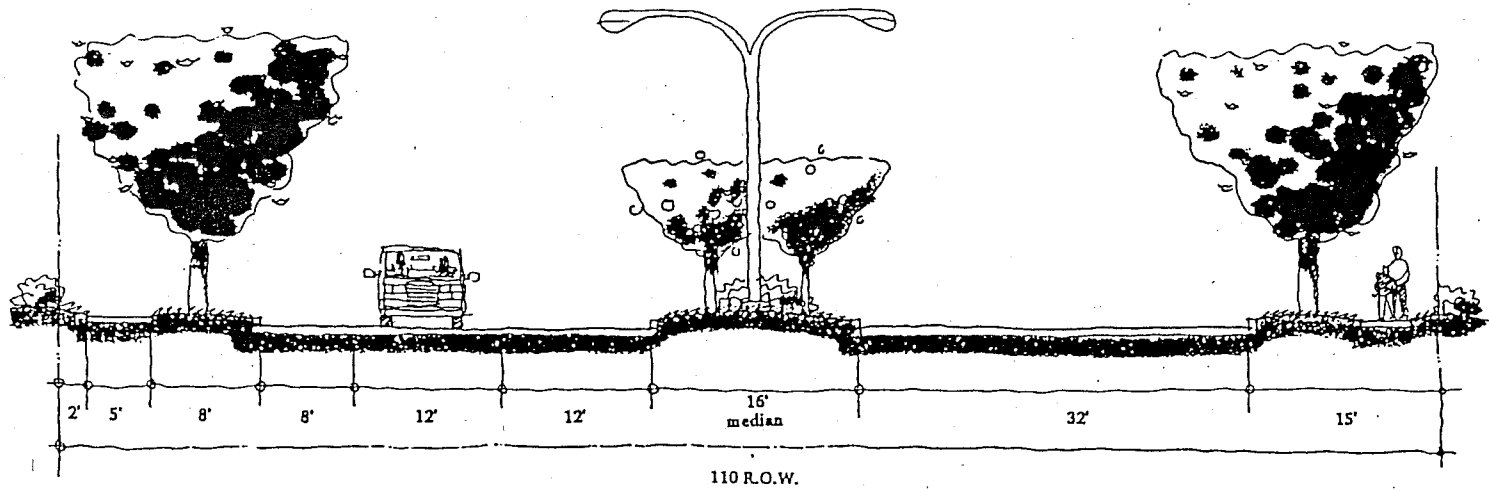


# Northeast Industrial

Figure 8B  
STREET SECTIONS



Phase 1 Industrial Street Section - preserves potential for expansion



Arterial Street

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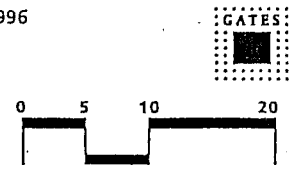


Table 9 provides a summary comparison of the Northeast Industrial Circulation Plan, the future expansion potential of the Plan, the RMP 35 year roadways and the RMP ultimate roadways.

*Table 9: Comparison Of Circulation Plans*

| Roadway Segment   | Northeast Plan                            |                                  | Roadway Master Plan                |                                    |
|---|---|----------------------------------|------------------------------------|------------------------------------|
|   | Proposed Circulation Plan                 | Potential Expansion              | 35 Year Scenario                   | Ultimate                           |
| Grant Line, between MacArthur and north-south through route | 4-lane arterial<br>(110 ft. ROW)          | 6-lane arterial<br>(110 ft. ROW) | 4-lane expressway<br>(116 ft. ROW) | 6-lane expressway<br>(140 ft. ROW) |
| Grant Line, east of north-south through route               | 4-lane arterial<br>(110 ft. ROW)          | 6-lane arterial<br>(110 ft. ROW) | 4-lane expressway<br>(116 ft. ROW) | 6-lane expressway<br>(140 ft. ROW) |
| North-south through route, south of Grant Line              | 2-lane industrial street<br>(110 ft. ROW) | 6-lane arterial<br>(110 ft. ROW) | 4-lane expressway<br>(116 ft. ROW) | 6-lane expressway<br>(140 ft. ROW) |
| North-south through route, between Grant Line and I-205     | 2-lane industrial street<br>(110 ft. ROW) | 6-lane arterial<br>(110 ft. ROW) | 4-lane expressway<br>(116 ft. ROW) | 6-lane expressway<br>(140 ft. ROW) |

#### Phasing Of Roadway Section

As discussed above, the Northeast Industrial roadway network does provide for right-of-way width that would accommodate a potential north-south major arterial connection to I-205. The Northeast CDP reserves a 110 foot major arterial right of way along the Chrisman / Paradise alignment for the future north-south connection. As shown in the Phase I Arterial section, the 40 foot median would be narrowed to 16 feet, thus creating an additional two 12 foot lanes. The temporary section of the median would be planted with low cost, low maintenance plantings, such as seeded groundcover or grasses.

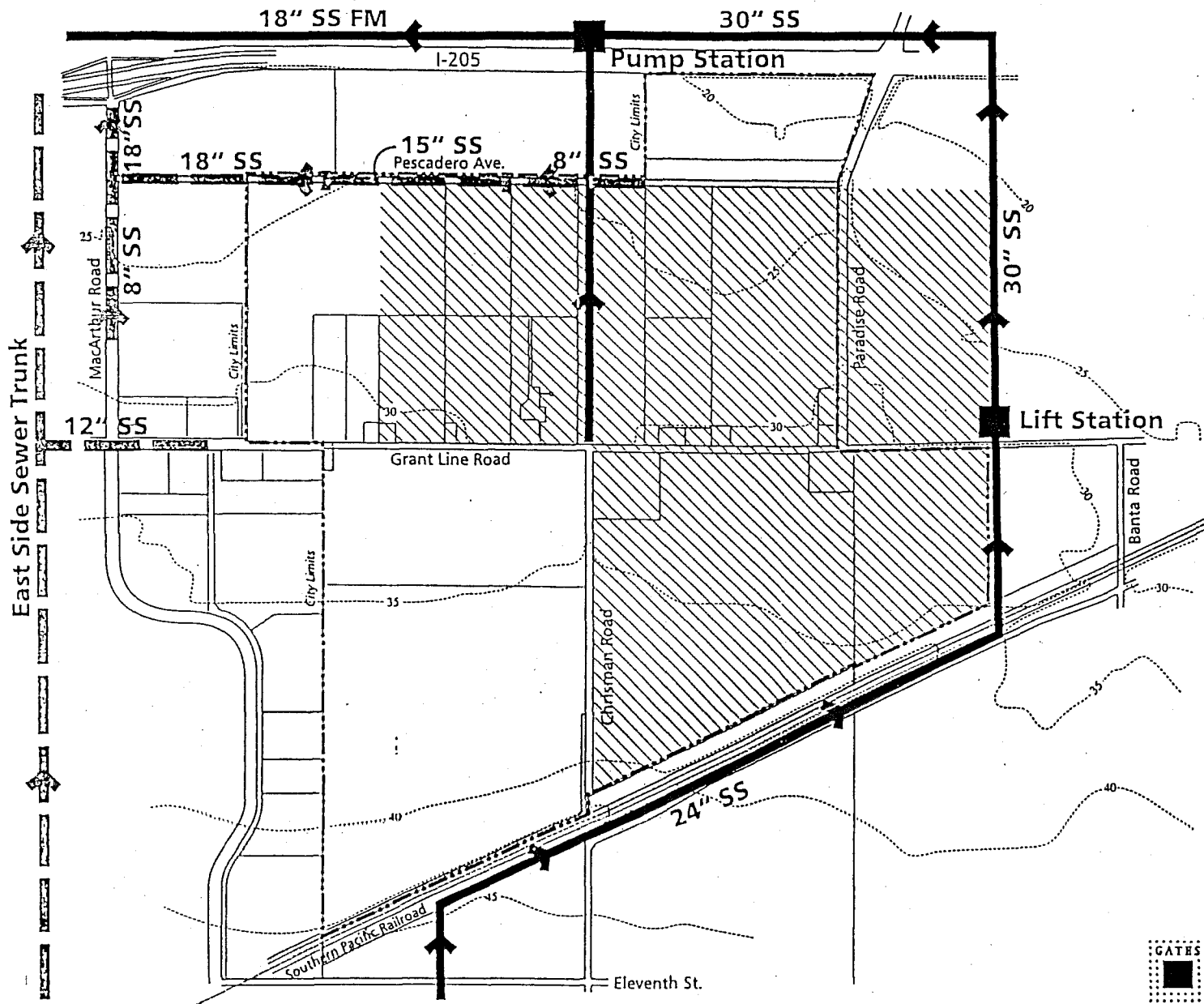
The section of Chrisman Road south of the loop street will be retained in an unimproved condition until needed as an arterial. A 110 foot wide right of way will be reserved to allow for the future expansion.

## PUBLIC SERVICES AND INFRASTRUCTURE

### Wastewater Collection and Disposal

#### City Master Plan

Figure 10 shows a cross hatched area which is identified on the City's Wastewater Master Plan as part of the tributary area for new sewer pipelines proposed by the Master Plan. Because the City's Master Plan does not provide any other information, it is assumed the remaining portion of the site, which is not cross hatched, is planned to drain into the East Side Trunk.






# Northeast Industrial

Figure 10

## MASTER PLAN SEWER

### LEGEND

-  Tributary Area for M.P. Sewer
-  Existing Sanitary Sewer
-  Master Plan Sanitary Sewer

Source: City of Tracy Wastewater Master Plan

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The City's Master Plan shows a new 18" diameter sanitary sewer to be installed from Grant Line Road at Chrisman Road, north through the Northeast Industrial planning area and through the Yellow Freight site (the main alignment is only conceptually shown on the Master Plan), then across I-205 to a new pump station and force main which would convey flows westerly to the treatment plant. It appears that most, if not all, of the cross hatched portion of the Northeast Industrial planning area is intended to be served by this new system. If this main is to be built, it would be constructed east of Yellow Freight.

The Master Plan also shows a future 24" and 30" sewer system flowing counter-clockwise generally around the southern, eastern and northern boundaries of the site and then into the pump station described above. It appears this system is primarily planned to serve other areas, however the City's Master Plan does not provide any detail about shed boundaries, and this system may also be intended to serve some portion of the North East Industrial site.

### **Sanitary Sewer Serviceability Analysis**

Figure 11 shows a proposed sanitary sewer pipe system layout to serve each parcel within the North East Industrial site. This proposed collection system differs from the City's Master Plan and would discharge the entire Northeast Industrial planning area into the East Side Trunk. If found to be a viable alternative, this may eliminate the need for Northeast Industrial to construct significant portions of the new systems shown on the Master Plan, particularly the new pump station and 18' force main on the north side of I-205.

Preliminary analysis of pipe grades and USGS topography maps indicate it may be possible to serve the entire Northeast Industrial planning area with a gravity system which outfalls to the East Side Trunk. The proposed layout shown on Figure 11 would make use of the existing pipes in Grant Line and Pescadero Roads to the extent possible. However, due to vertical considerations, it may be necessary to bypass some or all of the existing pipes leading to the East Side Trunk (particularly the 8" pipe in Pescadero Avenue). It should also be noted the area which the existing pipe in Grant Line Road can serve is probably limited, due to grade restrictions caused in part by conflicts with other utilities.

Depending on the calculation methodology used for sewage generation, inflow, infiltration and peaking factors, and the assumptions used regarding specific land use and employee density, the Northeast Industrial planning area is expected to have a sewage generation of between 1.5 and 3.0 million gallons per day (mgd). The 3.0 mgd could be exceeded if a high water user is located within the site.




A recent draft report by CH2M HILL shows the excess capacity of the East Side trunk to be about 3.6 (mgd) downstream of Grant Line Road. Preliminary calculations indicate the existing 18" sewer main in Pescadero Avenue has a total capacity of about 2.4 mgd and its excess capacity is estimated at 1.8 mgd (current flows in this system were not available from the City Public Works Department and should be verified). The portions of the existing excess capacities in the East Side Trunk and tributary systems, which can be used by the Northeast Industrial planning area, needs to be determined in order to assess the viability of the layout shown in Figure 11.

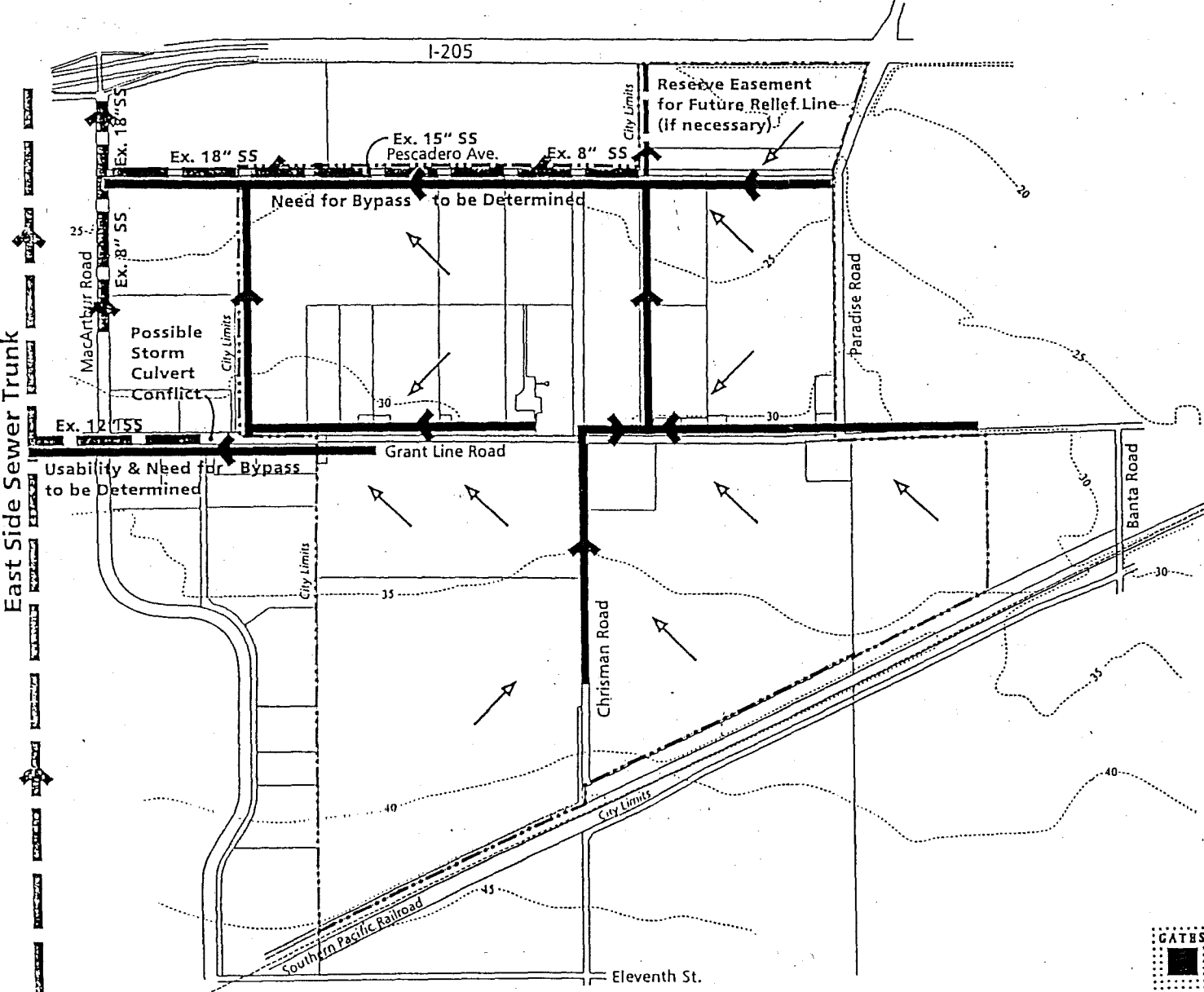
# Northeast Industrial

Figure 11

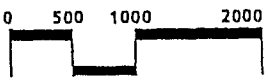
## PROPOSED SEWER SYSTEM

### LEGEND

-  Existing Sanitary Sewer Main
-  Proposed Sanitary Sewer Main
-  Direction to Main Connection for Development Areas



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If it is determined that in the ultimate buildout condition there will not be enough excess capacity in the East Side Trunk to serve the entirety of the Northeast Industrial planning area, it may be possible to temporarily serve a significant portion of the Northeast Industrial buildout via the East Side Trunk. This would allow construction of expensive offsite improvements, such as the new pump station and force main, to be deferred until a later date. A potential capacity shortage could occur as a result of capacity being allocated to others or the siting of a high water use facility within the plan area. It is suggested that easements for additional mains flowing northerly be reserved to allow for future flexibility in providing alternative sewer main scenarios.

If necessary, other alternatives to the City's Master Plan which may be worthy of consideration are to make use of the Heinz pipeline which may be abandoned in the near future and/or to parallel the East Side Trunk.

## **Storm Drainage**

### **City Master Plan**

As shown on Figure 12, the City's Master Plan identifies a main drainage divide through the middle of the site, approximately along the Chrisman Road alignment. Lands to the west of this divide are planned to flow to the west towards the East Side Channel. Lands to the east of this divide are planned to drain to the east into a future channel which would flow to the north and then north-west paralleling Tom Paine Slough to Sugar Cut.

### **Storm Drain Serviceability Analysis**

The City's current Storm Drain Master Plan for areas east of Chrisman Road will require expensive offsite improvements, and as such, alternative Master Plan drainage solutions for the Northeast Industrial planning area have been considered. Alternatives which merit consideration include the following:






- (a) Drain some or all of the portion of the site which is east of Chrisman Road to the East Side Channel. Due to capacity issues in the East Side Channel, any such shed "diversion" will result in the need for a detention basin(s) on site. The location and depth of detention basin(s) and channel hydraulics will dictate whether or not pumps will be necessary to drain the pond(s). Groundwater considerations will also influence the size of the ponds.
- (b) Drain the eastern portion of the site directly north to the Tom Paine Slough in a newly constructed drainage facility. This alternative may also require one or more detention basins and/or water quality basins. This will require a revision to the Master Plan and approvals from those responsible for the operation of Tom Paine Slough.
- (c) Temporarily discharge the eastern portion of the site into existing irrigation/tail-water ditches and await the construction of ultimate drainage facilities. This may require detention if temporary facilities are limited in capacity.
- (d) Temporarily store water in onsite retention basins and await the construction of ultimate drainage facilities. This concept may include temporary discharge to the East Side Channel or the tailwater ditches. Temporary facilities will need to follow a phasing plan.

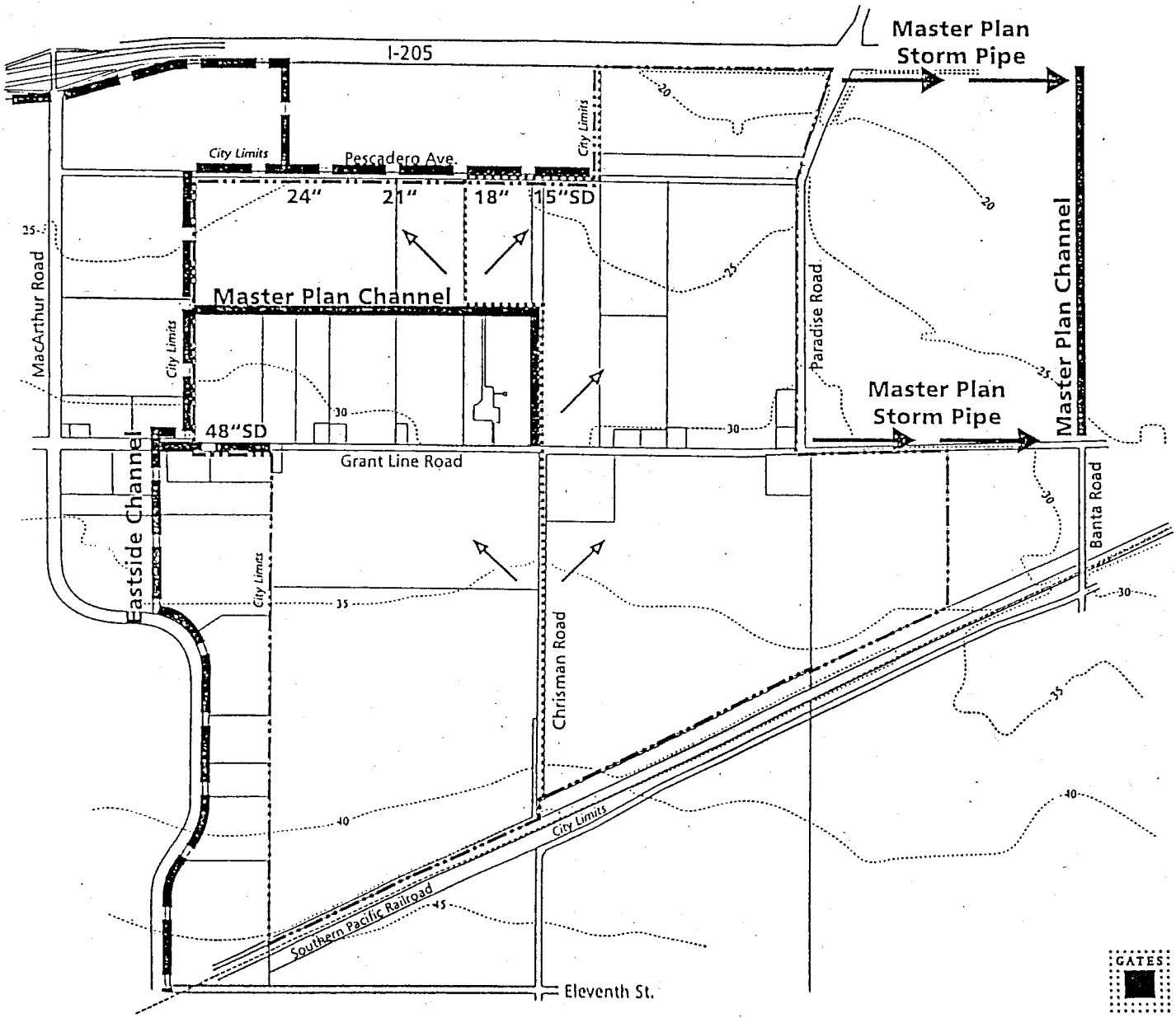
# Northeast Industrial

Figure 12

## MASTER PLAN STORM DRAIN

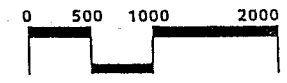
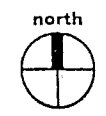
### LEGEND

-  Drainage Boundary
-  Direction of Drainage
-  Existing Storm Drain
-  Master Plan Storm Pipe
-  Master Plan Storm Channel



Source: City of Tracy Storm Drainage Master Plan

February 26, 1996



As properties within the project area develop, tailwater drainage from the West Side Irrigation District will need to be contained, redirected, or otherwise accommodated. The details of how this accommodation might best occur need further study.

## **Water Supply and Distribution**

### **Water Supply**

The Northeast Industrial Area will be annexed to the City of Tracy's water service area. Currently, the City uses a blend of treated surface water and untreated groundwater. The City of Tracy Water Master Plan currently proposes using surface water sources exclusively to meet water supply demands. The Master Plan indicates that existing groundwater supply will be maintained only for emergency usage. The City plans to increase their water treatment capacity to both meet the increased demand associated with the projected population growth and to allow additional surface water supplies to be used in lieu of the current groundwater supply.

### **City Master Plan Distribution Network**

The City Water Master Plan, a portion of which is shown on Figure 13, shows a network of mains varying in diameter from 12" to 18" throughout the site. A 12" to 16" diameter main is planned along Grant Line Road, to the east end of the site, and beyond. An 18" diameter water main is planned to run along Chrisman Road. A 12" diameter main is planned along Paradise Road. The Master Plan also calls for a 12" diameter water main loop in the southeast portion of the Plan Area.

### **Water System Serviceability Analysis**

Figure 14 shows a likely refinement to alignments shown on the City's Water Master Plan which takes into consideration the proposed Northeast Industrial roadway network. The size and location of water lines will need to be reviewed as development plans for the site and surrounding areas progress.

### **Police Protection**

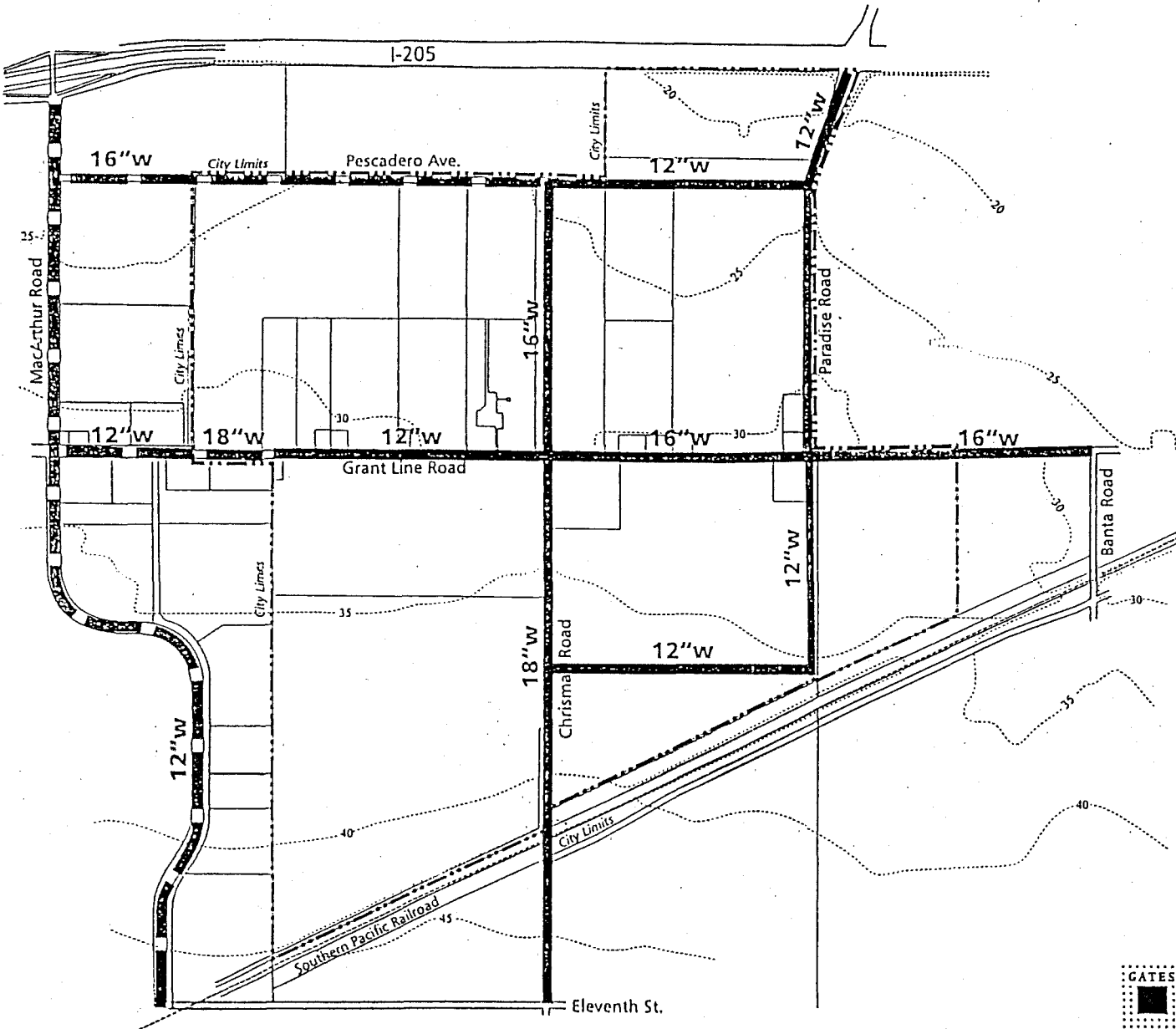
Police services to the project will be provided by the City of Tracy Police Department.

### **Fire Protection**

The planning area is located within the boundaries of the Tracy Fire Department (TFD). TFD will provide fire protection service and paramedic ambulance service to the planning area.

### **Other Utilities**

Pacific Gas and Electric (PG&E) will provide electricity and natural gas to the planning area. Pacific Bell will provide telephone service to the planning area from the Central Office located in Tracy.





# Northeast Industrial

Figure 13

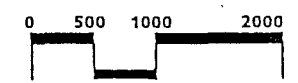
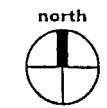
## MASTER PLAN WATER

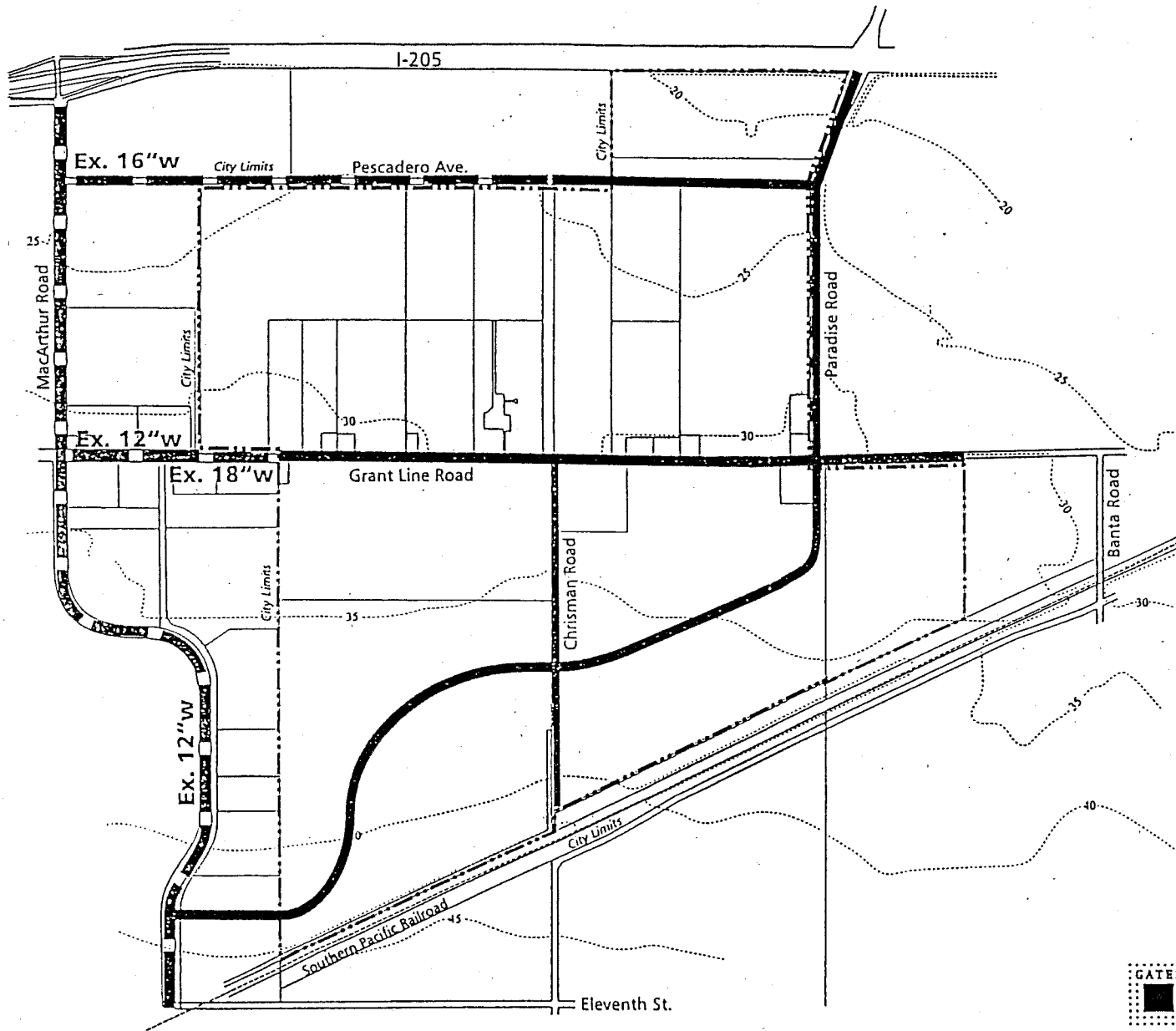
### LEGEND

-  Existing Water Main
-  Master Plan Water Main

Source: City of Tracy Water Master Plan

February 26, 1996







# Northeast Industrial

Figure 14

## PROPOSED WATER SYSTEM

### LEGEND

-  Existing water Line
-  Proposed Water Line

February 26, 1996



## PHASING AND FINANCING

### Phasing

The presence of the existing dairies and residences leads to a natural division of the Northeast Industrial planning area into several phases.

### Financing and Fiscal

The proponents of the Northeast Industrial Plan, like the I-205 Corridor Specific Plan and a number of other projects before it, will work with City Staff to arrive at a plan to finance the infrastructure needed to serve the project. Toward that end, a draft Financing Plan will be submitted within a reasonable time of submitting this document. This Financing Plan will identify what facilities are likely to be needed, what their estimated costs are, and how they might be paid for. It is anticipated that this plan will include a combination of impact fees and community facility districts to pay for the improvements over time. It is not considered likely that the project will cause a fiscal impact to the City that cannot be funded within the provisions of the property tax rate.



## DESIGN GUIDELINES

The Conceptual Development Plan includes design guidelines and development standards to guide site planning and architecture. These design guidelines are essentially the same as those adopted in the Industrial and I-205 Specific Plans, which are adjacent the planning area.

### Streetscapes

1. The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, berming, lighting, and signage to produce an attractive and functional environment.
2. All landscaping should employ a mix of trees, shrubs, groundcovers and turf where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs and turf is encouraged.
3. The use of lawn substitutes is encouraged in all medians and for parkways. The use of turf should be minimized and reserved for areas of high use or visibility and temporary median planting in anticipation of future street widths.
4. Automatic irrigation is required for all landscape areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, parking areas, etc. The use of water conserving systems such as drip irrigation for shrub and tree planting is encouraged.
5. Tree plantings should reflect street hierarchy with larger trees along arterial streets and smaller trees on industrial streets. Tree plantings shall be symmetrical and of the same species in the parkways on both sides of the streets. One tree species or mixture of species shall be planted consistently at regular intervals along the entire length of a street. Spacing interval shall be no greater than 40 feet on center. Where trees are planted in medians, the plantings shall be continuous and at regular intervals. Spacing of median trees shall be no greater than 30 feet on center. Different tree species shall be planted at intersections to highlight these areas.
6. Adequate sight lines shall be maintained at intersections.

### Recommended Trees for Major Streets

The following list identifies recommended trees for the major streets that form the framework of the area:

| <i>Street</i>        | <i>Parkway</i>                               | <i>Median</i>                                    |
|----------------------|--|--|
| Grant Line Rd.       | Pistacia chinensis<br>Chinese Pistache       | Prunus serrulata cvs.<br>Flowering Cherry        |
| Paradise Rd/Loop Rd. | Celtis sinensis<br>Chinese Hackberry         | Pyrus calleryana 'Bradford'<br>Bradford Pear     |
| Pescadero Avenue     | Fraxinus oxycarpa 'Raywoodii'<br>Raywood Ash | Pyrus calleryana 'Aristocrat'<br>Aristocrat Pear |

Select one of the following street trees for use on each industrial road:

| <i>Botanical Name</i>         | <i>Common Name</i> |
|-------------------------------|--------------------|
| Eucalyptus gunnii             | Cider Gum          |
| Fraxinus oxycarpa 'Raywoodii' | Raywood Ash        |
| Fraxinus uhdei                | Evergreen Ash      |
| Platanus acerifolia           | Sycamore           |

**Street Lighting**

1. Illumination standards for arterial and industrial streets should reflect the different right-of-way widths and functions.
2. Light fixtures and standards shall meet all safety standards and be employed throughout the length of each street. It is recommended that one lighting fixture style be employed for use on all streets. The preferred style is a Davit pole painted grey green.
3. Where possible, light standards shall be located in median.

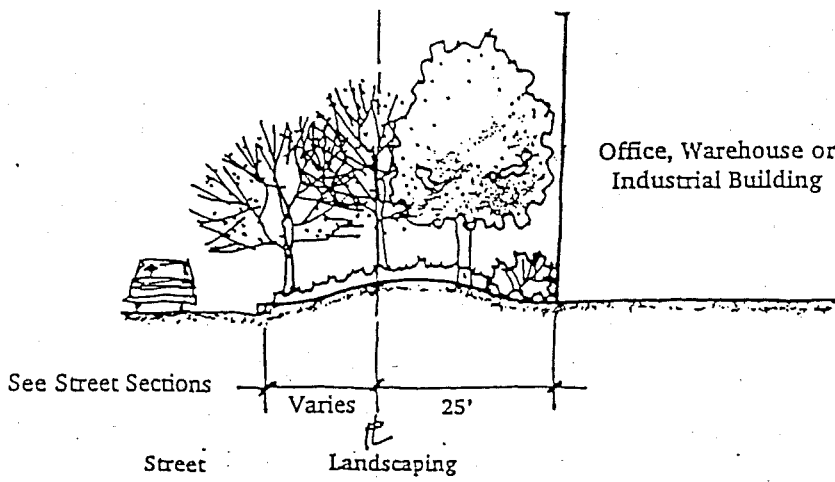
**Building FAR and Height**

| <i>Land Use</i>    | <i>Max. Floor Area Ratio</i> | <i>Max. Bldg. Ht.</i> |
|--------------------|------------------------------|-----------------------|
| General Commercial | 0.35                         | 40 ft.                |
| Office             | 0.35                         | 40 ft.                |
| Light Industrial   | 0.50                         | 40 ft.                |

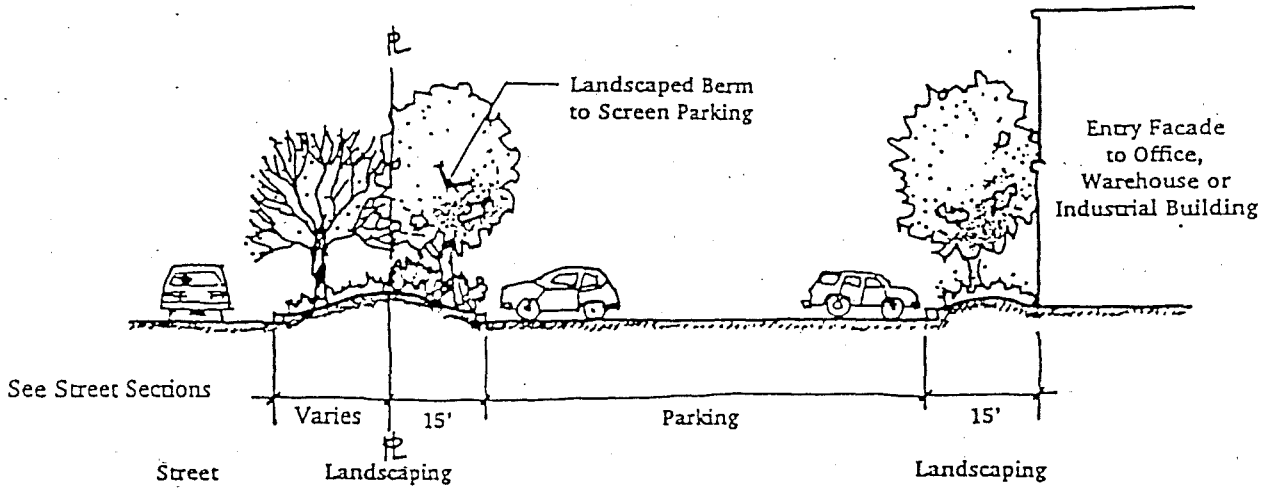
**Building Setbacks**

The following shall be the minimum building and parking setbacks required for all building types. Figure 15 illustrates these guidelines.

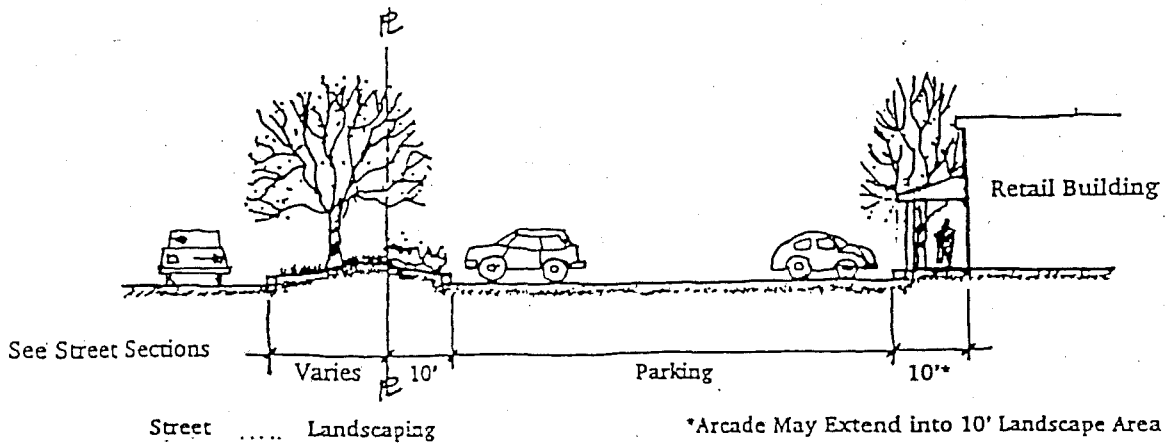
1. Building setback from any property line adjacent a street or CalTrans right of way shall be 25 feet minimum. Rear and sideyard building setbacks from property lines not adjacent street or CalTrans right of way shall be 15 feet minimum.
2. A 5 foot wide landscape setback is required along property lines not adjacent to a right-of-way. On the property lines perpendicular to the street frontage on industrial sites, the landscaped setback is only required to a point 150 feet onto the parcel from the street right-of-way or 50' back of building face, whichever is greater.
3. Parking setback from any property line along a public street or the Cal Trans right-of-way, for commercial shall be 10 feet and for industrial shall be 15 feet.
4. Parking shall not be permitted within 10 feet of the building entry face of any commercial structure. In the event the building has an arcade or other shade structure along this frontage, the structure can be located within this required setback. Parking shall not be permitted within 15 feet of the office face or portion of a building. On industrial buildings a 15 foot setback to parking shall be provided at building entries.



Front Yard Setback: Without Parking



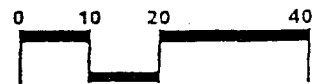
Front Yard Setback: With Parking



Front Yard Setback: With Parking

Figure 15

# SETBACK GUIDELINES



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5. Commercial buildings must shall be sited so as to create and enhance the streetscape. This can only be accomplished if all or a portion of the buildings are located near the street. On commercial sites of over 4 acres, at least one building must be located with a minimum setback from public right-of-way to building face of 50 feet. On corner sites adherence to this requirement is encouraged on both frontages, however only required on the major street frontage.

**Parking and On-Site Vehicular Circulation**

1. Parking, on site circulation and loading area standards shall be as required by the provisions of Section 10-2.26, Off Street Parking Requirements, of Tracy Municipal Code unless modified below or as part of the Final Development Plan approval. Portions of off-street parking requirements are summarized below.
2. Parking lots containing 10 - 20 spaces may include a maximum of twenty (20%) percent of the total number of spaces for compact cars (these spaces shall be designed and marked in accordance with City Standards and distributed throughout the lot.) Compact car space parking areas containing twenty (20) or more spaces may include a maximum of thirty (30%) percent of the total number of spaces for compact cars.
3. Minimum Off Street Parking Standards:

| <i>Uses</i>  | <i>Minimum Parking Spaces Required</i>   |
|--|--|
| Retail   | One space per 250 square feet of gross floor area  |
| Vehicle sales and rentals, including recreational vehicles and mobile homes                    | One space per 250 square feet of gross floor area plus one space per vehicle for sale or stored on lot.  |
| Offices: business, professional (not including medical or dental), banks                       | One space per 250 square feet of gross floor area.   |
| Dental and medical clinics or offices  | One space per 200 square feet of gross floor area.   |
| Motor vehicle repair garages   | One space per 600 square feet of gross floor area; repair stalls not counted as parking spaces   |
| Cafes, restaurants and other establishments for the sale and consumption of food and beverages | Dining: one space per 45 square feet of customer area and one space per 250 square feet of all other area, plus additional spaces connected to uses such as drinking establishments.<br>Drinking bars, cocktail lounges: one space per 35 square feet of drinking, bar, lounge area. |
| Manufacturing  | One space per 600 square feet of gross floor area, or if the number of employees on the maximum work shift can be verified, one space per one employee on the maximum work shift   |

| <i>Uses</i>                         | <i>Minimum Parking Spaces Required</i>   |
|-------------------------------------|--|
| Warehouses/Storage and Distribution | One space per 1,000 square feet of the first 20,000 square feet of gross floor area, plus one space per 2,000 square feet of the second 20,000 square feet of gross floor area, plus one space per 4,000 square feet of the remaining square feet of gross floor area. |

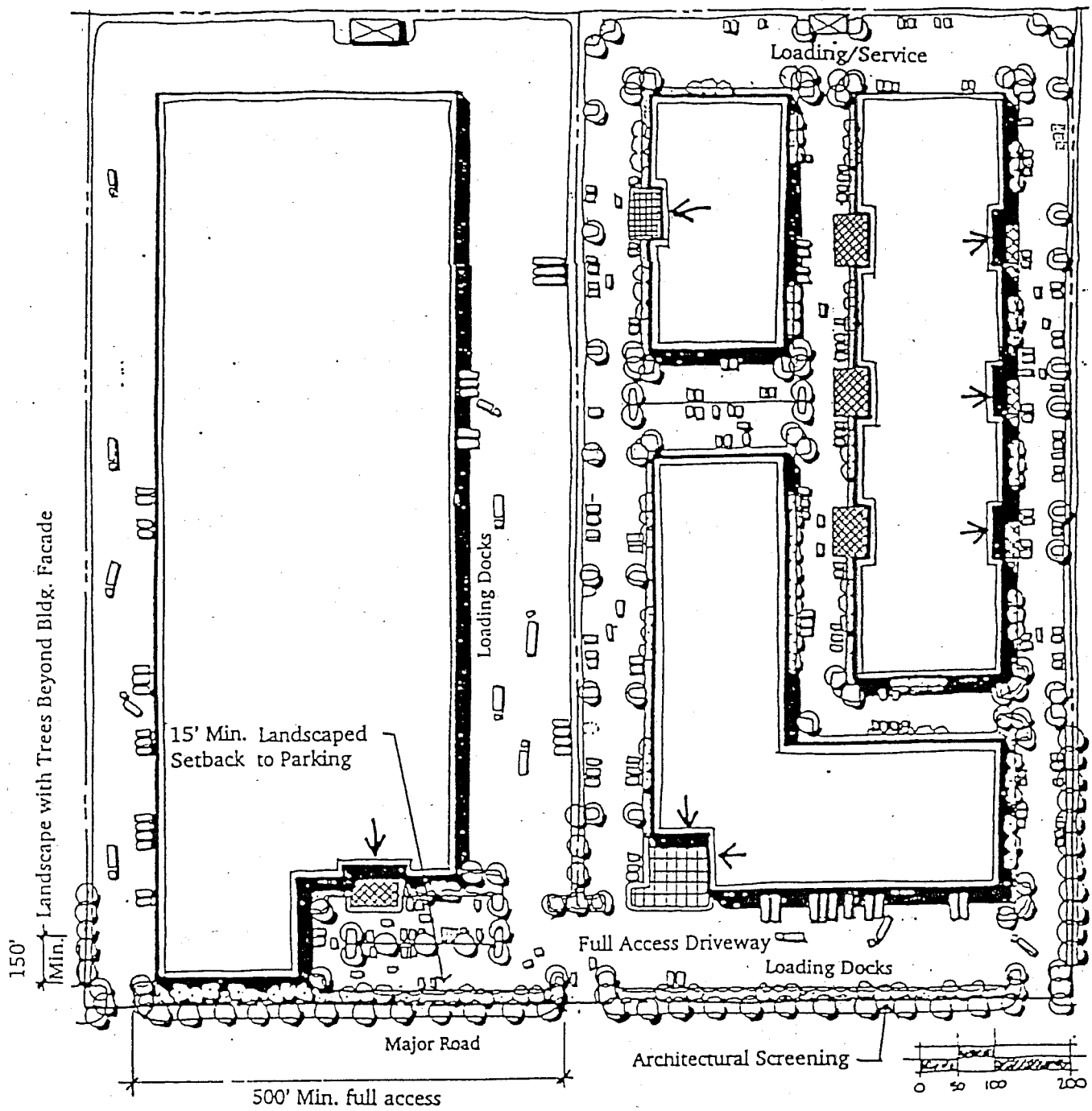
### Loading and Unloading Spaces

1. Sufficient off-street loading and unloading spaces shall be provided on each site, and adequate provisions and space shall be made for maneuvering freight vehicles and handling all freight. All loading activity, including turnaround and maneuvering, shall be made on-site.
2. In commercial areas, truck loading areas and docks shall not be permitted between building(s) and the public street unless enclosed with architectural screen of material similar to building.
3. In industrial areas, truck loading areas and docks shall not be permitted between building(s) and the street unless the building(s) are set back from the curb a minimum of 125 feet and doors are screened by landscaping, berms and/or fences.
4. Buildings, structures and loading facilities shall be designed and placed upon the site so that vehicles, whether rear loading or side loading, may be loaded or unloaded at any loading dock, door, or area, without extending beyond the Property Line.

### Driveway Standards

Driveways should be carefully located so as not to impede the primary function of the streets, which is to carry through traffic. It should be noted that these spacing guidelines are minimum values. The goal should be to exceed them where possible.

1. Individual industrial parcels on major arterial streets may have driveways, but they should be carefully located so as not to impede the traffic efficiency. In general, parcels with frontage on major arterials should have their driveway on side streets if possible. If a parcel's only frontage is on the major arterial, every effort should be made to consolidate access at a single driveway. Spacing standards for driveways on major arterials shall be as follows:
  - a) full access driveways, 500 ft. minimum
  - b) partial access driveways (right in / out, left turn in), 500 ft. minimum
  - c) right turn in and out, 350 ft. minimum upstream from an intersection
  - d) right turn in and out, 200 ft. minimum downstream from an intersection
2. On industrial streets, spacing for full access driveways is 450 feet, minimum. "T" intersections are encouraged over four-way intersections. Every effort should be made to consolidate driveways.
3. No driveway shall be located closer than 200 feet to the radius return point at intersections.



Warehouse / Storage / Distribution  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 213,000 s.f.  
 73 Parking Stalls

Light Manufacturing / R&D  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 193,000 s.f.  
 429 Parking Stalls

Figure 16

# LIGHT MANUFACTURING / R&D PROTOTYPE



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4. Driveways shall be a minimum of 25' wide. Subsequent development applications shall demonstrate driveway width and placement can accommodate truck turning movements and clearing without blocking roadways.
5. Driveway width modifications may be approved with shared (ganged) driveways. Ganged driveways which serve two adjacent sites will be required to install landscaped islands along parking adjacent to the gang driveway and a landscape zone at the end of the common drive will act as a terminus to the view line down the ganged driveway.
  - a) Full curb returns (as opposed to a standard driveway) shall be utilized for entries to all sites of over ten acres in size, or for common driveways that serve two adjacent sites that together total more than ten acres.
6. Access driveways shall provide adequate length to accommodate off-street vehicle stacking needs during times of peak use.
7. Parcel entry areas should be clear, attractive, and inviting; circulation should direct and visitor traffic clearly through the site, to main building entries and drop-off points, and service trucks to loading.
8. In commercial areas, vehicular entries to the site shall be well defined and recognizable to motorists. Improvements should include accent paving, signs, special plantings and lighting. Such improvements shall not block motorist's sight lines to oncoming traffic.

### Freeway Interface

The control of views of Tracy from Interstate 205 is critical for the establishment of a quality image for the community.

1. Locate service and store areas to minimize visibility from Interstate 205.
2. All freeway setback zones shall be planted with a combination of trees, shrubs, and groundcover. Automatic irrigation is required of all planted area. Use large scale trees, from a 15 gallon can minimum, grouped in single species clusters. Mass trees to avoid blocking views of commercial signage, while providing at least one tree per 1500 square feet of setback area. Plant shrubs in an informal hedge near the property line, with gaps between hedges of 50 feet maximum. Install from 1 gallon cans, minimum, in single species clusters at least 100 feet long. Hydroseed or otherwise install permanent groundcover in all places not planted with shrubs.

### Building Architecture

1. Use of creative building design and construction techniques is encouraged. Special attention should be given to that portion of the building visible from adjacent roadways or public parking areas.
2. Large buildings should have facades that include variations in massing, form and texture. Continuous surface treatments of a single material should be minimized. Architecture should be used to highlight building entries.
3. Any accessory buildings and enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.

4. Continuous arcades along the front of commercial buildings are encouraged as they provide the pedestrian protection from the weather, reduce solar gain and can serve to enhance the character of what might otherwise be simple, formless structures. When more than one building is to be developed on a commercial site the buildings should be designed to relate to one another as a total composition with well thought out relationships one to another.
5. Metal buildings shall only be allowed where the industrial nature of the use seems to mandate this type of construction. If metal buildings are found appropriate surface treatment to the office portion of such structure facing the public street shall be required.

## Signs

1. Signs must conform to the requirements of Signs, Section 10-2.35 of the Tracy Municipal Code as modified herein.
2. A site sign program shall be integrated into a total design concept for a site and its buildings. The primary goal of the project sign system is to provide information and identification. When more than one sign is permitted all signs shall be of similar style, shape and materials.
3. All signs must be approved as a part of the Final Development Plan. A sign program shall be submitted as a part of the application. The sign program should include:
  - a) **Detached Business Identification Signs:** One such monument sign (as defined by the Tracy Municipal Code) shall be allowed for each street frontage of the site. These signs may only contain the symbol and/or name of the business and its street address. The sign shall be free standing, may be double sided, and shall be set back a minimum of 5 feet from the public right-of-way. Sign area shall not exceed 32 square feet per frontage and sign shall not exceed 6 feet in height from finish grade. Signs should generally be oriented perpendicular to approaching traffic.
  - b) **Wall Signs:** On large single tenant buildings, signs should be located immediately above or adjacent to the primary building entrance. No sign shall extend above dominant roof line. The area of any single sign shall not exceed 100 square feet. Total area shall not exceed one-half square foot of sign per lineal foot of business being served.

On smaller multi-tenant buildings, signs should be located at the frontage of each individual lessee. The area of any single sign shall not exceed 100 square feet nor more than 75 percent of the tenant frontage. Capital letters shall be no more than 2.5 feet high and lower case letters no more than 1.5 feet in height. When individually-lettered wall signs comprise over 50 percent of the sign area of all sign types total sign area shall not exceed 1.2 square feet per lineal foot of business being served. When comprising less than 50 percent of the total sign area the maximum sign area shall be one half square foot per lineal foot of business being served.

- c) **Directional Signs:** Signs required or desired to assist patrons in accessing the facility shall be located in site parking areas. The design of such signs shall be simple and easily legible. There is no limit to the number of signs provided on a site, however no single sign shall exceed 6 feet in area, except that vehicular "stop" signs shall be mounted per State standards.



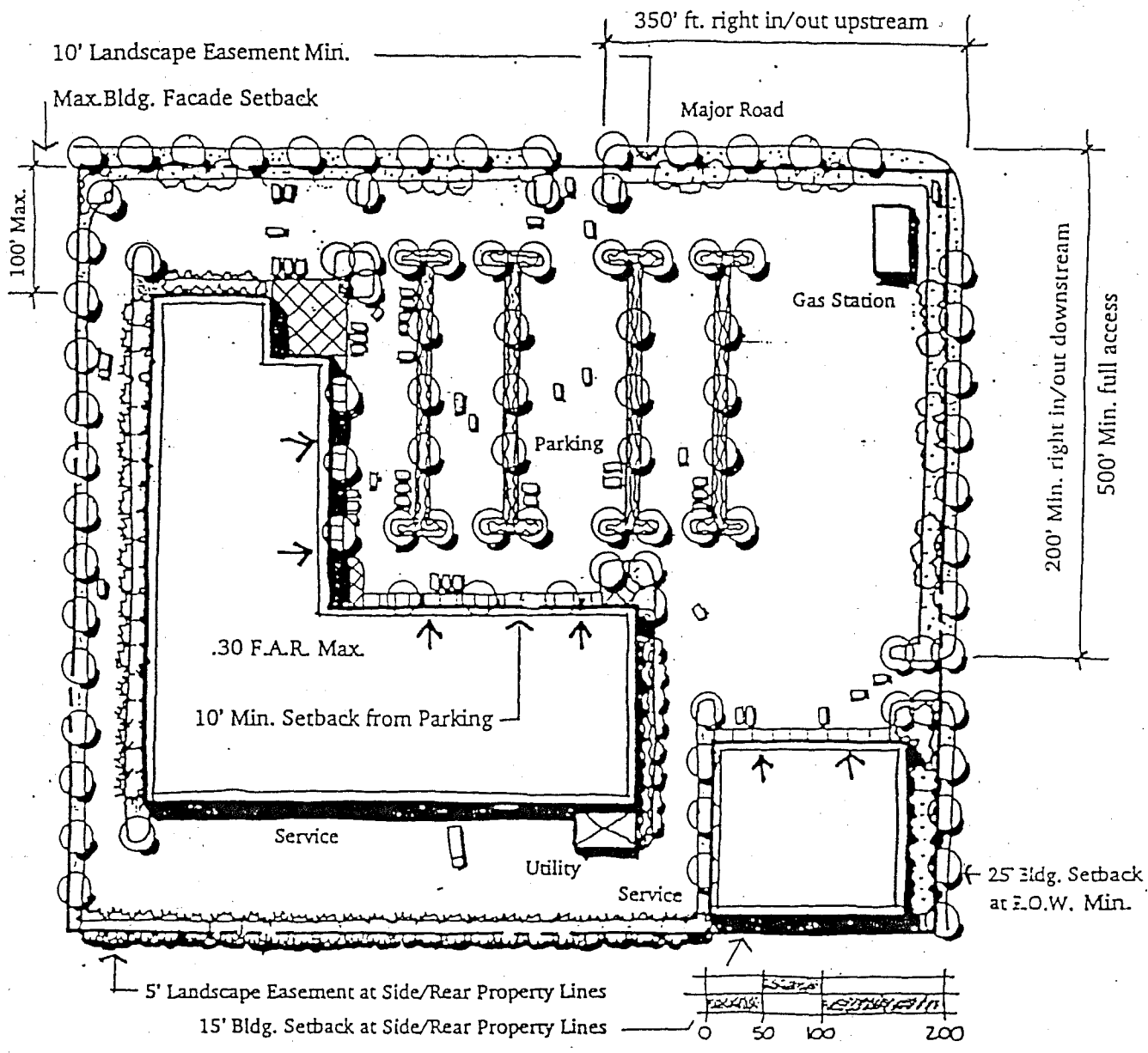


Figure 17

COMMERCIAL PROTOTYPE



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4. A sign may be illuminated provided that no flashing, traveling, animated or intermittent illumination shall be used. Such illumination shall be confined to the area of the sign except when such illumination is back lighting for an otherwise non-illuminated sign. No sign illumination shall cast a glare which is visible from any street.
5. Signs should be constructed with quality materials and in a craftsmanlike manner to ensure both an attractive appearance and a durability.

### Landscaping

Minimum on-site landscaping requirements shall be as established by Off Street Parking Requirements (Section 10 2.2613 of the Tracy Municipal Code), except as modified below.

| <i>SUMMARY OF REQUIREMENTS</i>              | <i>Commercial</i>   | <i>Industrial</i>    |
|---|---------------------|----------------------|
| Landscaped frontage setback                 | 10 feet             | 15 feet              |
| Minimum no. of trees in parking areas       | 1 tree per 5 spaces | 1 tree per 10 spaces |
| Percentage of landscaping in parking areas: |                     |                      |
| 0- 15 cars                                  | 5%                  | 5%                   |
| 16 - 30 cars                                | 10%                 | 5%                   |
| 31- 60 cars                                 | 15%                 | 7 ½ %                |
| Over 60 cars                                | 20%                 | 10%                  |

1. While commercial uses benefit from a well landscaped parking area and visibility from the street, views of industrial uses benefit from a more generously landscaped streetscape. Thus, parking lot landscaping requirements for industrial uses may be reduced as specified in the Tracy Off-Street Parking Requirements in order to create a large landscape setback along the street. These provisions allow the reduction of 50 percent of the required landscaping based on the providing a 15 foot landscape setback along the street frontage. The 15 foot strip may be included in the calculation of the total parking lot landscaping requirement. The remainder of the landscaping requirement must be distributed over the lot(s) to provide shade and landscape building frontage. Canopy trees shall be evenly distributed throughout the parking to provide shade.
2. On-site landscaping along arterials between the property line and the building, parking lot or vehicular circulation improvements shall be installed by the property owner. This landscaping shall be designed as an extension of the adjacent public arterial landscaping. Completion of landscaping on the site shall be simultaneous with completion of the building and other improvements on the site.
3. Landscaping shall not obstruct sight lines at street or driveway intersections.
4. In place of the wheel stops at parking lots, landscape areas and pedestrian walkways may be extended not more than two (2) feet into required parking spaces, to include a size (6") inch concrete curb. In such cases, no credit toward parking lot landscape requirements shall be given for the resulting additional landscaping.
5. Screening of the parking area from public rights-of-way in industrial areas shall be provided with a 2 ½ to 3 foot high element, measuring from the top of the parking area pavement. Screening may consist of one or a combination of the following:

- a) Berms landscaped with ground cover, trees, and shrubs;
  - b) Solid, low profile, decorative masonry walls;
  - c) Evergreen shrubbery which, when solely used as screening, shall be continuously maintained to provide solid screening.
6. Generous landscaping screening is required adjacent on all street frontages for industrial areas. These areas should be landscaped with a combination of trees, shrubs, and ground cover soften views of parking areas.
  7. Tree planting and selection and massing should be compatible with streetscape plantings. Provide minimum 1 tree per 400 sq. feet of landscape setback. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together
  8. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs and turf is encouraged. The use of turf in the narrow planting islands is discouraged.
  9. Live plant materials shall be used in all landscaped areas. The use of gravel, colored rock, bark and other similar materials are not acceptable as a sole groundcover material.
  10. All trees shall be of 15 gallon minimum size at planting, with a minimum branching height 5 years after installation of 10' above road or parking surfaces and 6' at pedestrian areas. Shrubs shall be of 1 gallon minimum size with a maximum on-center spacing of 24". Likewise, groundcover may be planted from flats or 4-inch pots with a maximum spacing of 12" on-center.
  11. Automatic irrigation is required for all landscaped areas. Irrigation systems should be designed so as not to overspray walks, buildings, and parking areas.

### Screening and Storage

1. All exterior trash areas, storage structures, and service areas should be screened from public view with a wall or fence of a minimum height of 8 feet above the street curb level. Storage areas shall be set back a minimum of 50 feet from streets, unless fully enclosed in an architecturally compatible enclosure.
2. No storage areas are allowed within the landscape easements, front setbacks or side or rear yard landscaped buffers.
3. Roof-mounted equipment shall be screened from street view. Pad-mounted transformers, utility connections, and meter boxes shall be screened and integrated into the site plan.
4. The design of masonry walls, fencing, trash enclosures and similar accessory site elements should be compatible with the architecture of the building and should use similar materials. Where masonry walls are along at property frontages, it should enhance the entrance to the property and should not impair traffic safety by obscuring views. Long expanses of wall surfaces should be architecturally designed to prevent monotony.

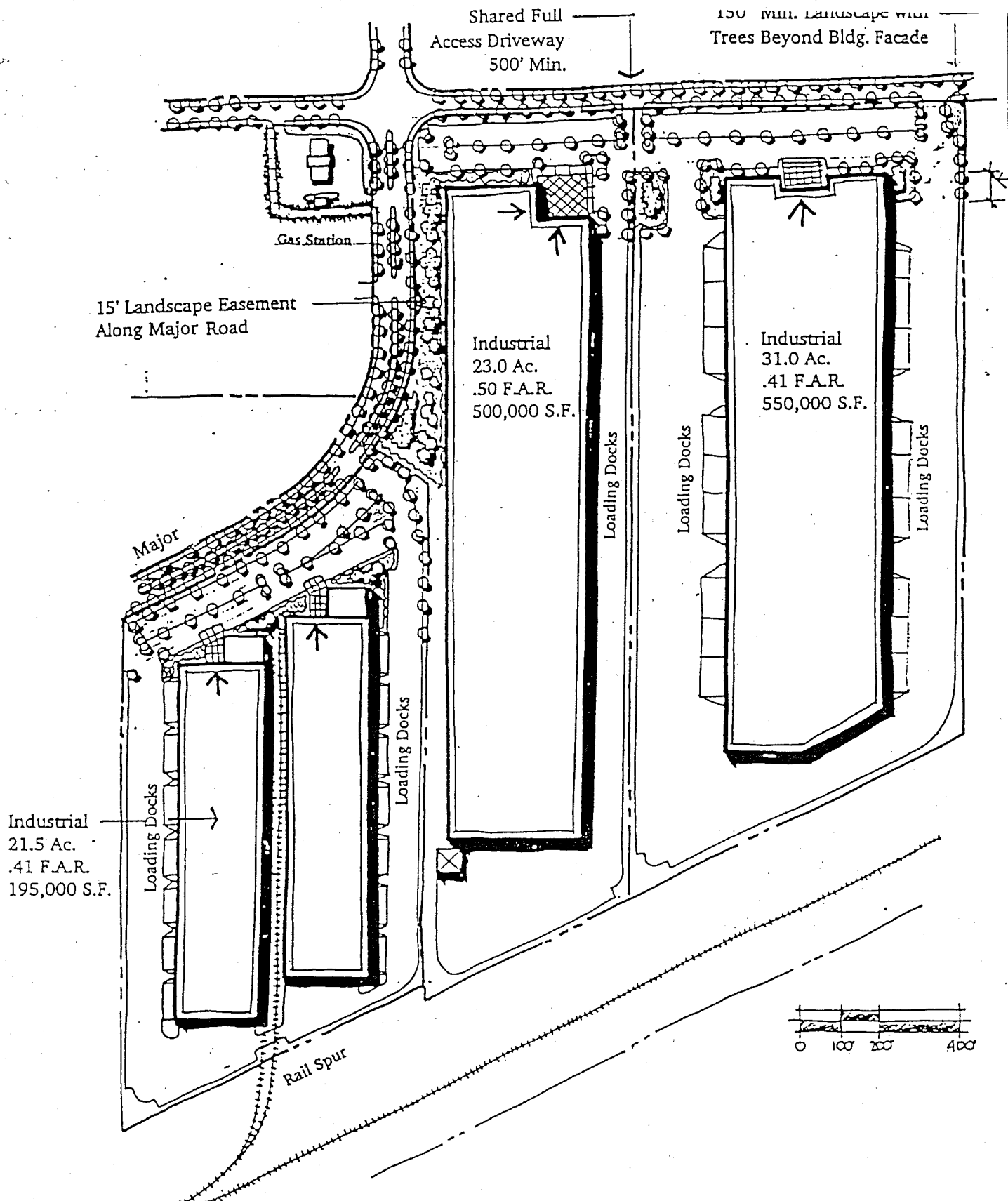


Figure 18

WAREHOUSE PROTOTYPE



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GATES



## Environmental Performance Standards

### Use Restrictions

No use shall be permitted to exist or operate on any lot which:

1. Emits dust, sweepings, dirt, cinders, fumes, odors, radiation, gases and vapors, or discharges liquid or solid wastes or other harmful matter into the atmosphere or any body of water which may, according to the appropriate agency, adversely affect the health and safety of persons within the area, or the health and safety of persons in adjacent areas, or the use of adjacent properties
2. Discharges waste or any harmful substance as defined by the Municipal Code, into any public sewer or storm drainage system.
3. Produces intense glare or heat, unless such use is performed only within an enclosed or screened area, and then only in such manner that glare or heat emitted will not be discernible from any exterior lot line.
4. Creates a sound pressure level in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
5. Allows the visible emissions of smoke (outside any building) other than the exhausts emitted by motor vehicles or other transportation facilities or any emissions in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
6. Creates a ground vibration that is perceptible, without instruments, at any point along any of the exterior lot lines.

### Hazardous Wastes and Water Pollutants

1. An on-site reconnaissance for hazardous wastes must be conducted for each parcel within the study area and the resulting report submitted with the application for the first proposed Tentative Map. If hazardous wastes are identified they must be dealt with to the satisfaction of the Tracy Municipal Code, before the application may be approved.
2. All new industries locating with the area will be required to obtain a Discharge Permit from the Director of Utilities prior to occupancy. This permit shall establish the amount and quality of wastes allowed to be discharged into the City's sanitary sewer.
3. The quality of wastewater entering the city sewage system from proposed uses shall be measured by the Biochemical Oxygen Demand (BOD) and Total Suspended Solids (TSS) levels referenced in the local Water Quality Control Board 208 Plan. Users that are not expected to comply with these standards will be required to provide on-site pretreatment facilities.
4. The storage and distribution of hazardous materials shall be subject to the rules of the San Joaquin County Health District.
5. Industries regularly using significant quantities of hazardous chemicals as defined by State Law in the course of their operations shall be required to obtain a Conditional Use Permit.

**Mineral Extraction**

1. Mineral extraction shall only be permitted as a subsidiary use to main use of site.
2. Locate equipment, storage, and facilities for mineral extraction to avoid visibility from the public street.

**Dairy Uses**

1. With development applications for an individual site, provide information demonstrating provisions of adequate buffers between proposed development and adjacent existing dairy uses.
2. Existing dairy uses are exempted from the Environmental Performance Standards on page 42.

**Permitted and Conditional Uses**

The following table indicates uses which are to be permitted and permitted subject to conditions in each land use designation of the Northeast Industrial Plan. The land use designations are abbreviated as follows:

- LI: Light Industrial
- GC: General Commercial

The uses shown in Table 19 as Permitted ("P") are those that are deemed acceptable anywhere in the assigned land use designation. They are uses that, when developed in conformance with this Plan, will not require special conditions in order to avoid negative impacts.

The uses shown as Conditionally Permitted ("C") are of two types. Some of these uses are not acceptable in every location within a land use, but are acceptable in certain locations. Other uses may require special conditions to make them acceptable at particular locations, due to their potential negative impacts on existing or planned uses. This may be because of their potential nuisance aspects such as litter, noise, or hazardous wastes.

Where neither a "P" or "C" is shown for a particular land use district, that use is not allowed.

*Table 19: Permitted and Conditional Industrial and Commercial Uses*

| <i>Uses</i>  | <i>LI*</i> | <i>GC</i> |
|--|------------|-----------|
| 1. Agricultural, including dairies   | P ♦        | P *, ♦    |
| 2. Accessory uses and structures; not including warehouses, located on the same site as a permitted use. | P          | P         |
| 3. Administrative, executive, research, medical offices  | P          | P         |
| 4. Accessory uses and structures located on the same site as a conditional use                           | C          | C         |
| 5. Warehousing and distribution facilities   | P          |           |

| Uses  | LI* | GC |
|---|-----|----|
| 6. Manufacturing, repair, assembly, or packaging of products from previously prepared materials, such as cloth, plastic, leather, or semi-precious metals or stones, but not including such operations as saw or planing mills, any manufacturing involving primary production of wood, metal, or chemical products from raw materials. | P   |    |
| 7. Manufacture, of food products, pharmaceuticals, biotechnology products and the like, but not including fish or meat products, sauerkraut, vinegar, or the like, or rendering or refining of fats and oils.   | P   |    |
| 8. Laboratories, including chemical, physical material testing, electronic, agricultural, photographic film processing, and general research  | P   |    |
| 9. Electrical industrial apparatus manufacturing, service, and repairs, including motors, generators, welding equipment, electrical transmission and distribution equipment, and turbines and pumps.  | P   |    |
| 10. Manufacture, repair of optical electronic, timing, and measuring instruments  | P   |    |
| 11. Dairy products plants   | P   |    |
| 12. Machine Shops   | P   |    |
| 13. Heating, plumbing, and ventilating equipment manufacturing, servicing, repairs  | P   |    |
| 14. Refrigerator, furnace, water heater, and other household appliance manufacturing, service and repairs, not incidental to retail sales.  | P   |    |
| 15. Furniture and cabinet assembling whose activities are carried on entirely within an enclosed building and which have no construction yards on the lot.  | P   |    |
| 16. Parcel delivery service and vehicle storage inside and outside the building   | P   |    |
| 17. Truck Terminals   | P   |    |
| 18. Mini storage  | P   |    |
| 19. Equipment Storage   | P   |    |
| 20. Janitorial services and supplies  | P   |    |
| 21. Printing, including also lithographing, engraving and other such similar reproduction services  | P   |    |
| 22. Automotive supply stores  | C   |    |
| 23. Rental yards, including the rental of hand tools, garden tools, power tools, trucks, trailers, and other similar equipment  | C   |    |
| 24. Building materials sales, lumberyards (outside storage)   | C   |    |
| 25. Repair, painting, and body work for automotive, motorcycle, and farm machinery  | C   |    |

| <i>Uses</i>  | <i>LI*</i> | <i>GC</i> |
|--|------------|-----------|
| 26. Boat sales, service, repair  | C          |           |
| 27. Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, tobacco, soft drinks, candy and gum. | C          |           |
| 28. Wholesale trade businesses   | C          |           |
| 29. Intermediate manufacturing uses involving the processing of raw materials, including food and paper processing, wineries, concrete mixers  | C          |           |
| 30. Mineral and hydrocarbon extraction   | C          |           |
| 31. Recycling (collection and sorting)   | C          |           |
| 32. Outlet stores and centers  |            | P         |
| 33. Warehouse retail, including furniture, office supply, sporting goods, or wholesale merchandising   |            | P         |
| 34. Hardware stores, including garden centers  |            | P         |
| 35. Home Improvement and Interior Decorating stores, including carpet, drapery, floor covering, paint, glass, and wallpaper shops  |            | P         |
| 36. Garden centers, including plant nurseries and retail sales   |            | P         |
| 37. Sporting goods and toy stores  |            | P         |
| 38. Specialty retail stores selling those items and services normally sold in department stores (including clothing, shoes, and accessories)   |            | P         |
| 39. Shoe and clothing repair   |            | P         |
| 40. Food markets, convenience markets  |            | P         |
| 41. Liquor stores  |            | P         |
| 42. Drug stores and prescription pharmacies  |            | P         |
| 43. Restaurants, including fast food   |            | P         |
| 44. Art galleries and artists supply stores  |            | P         |
| 45. Barbershops, beauty shops, and hairstylists  |            | P         |
| 46. Financial institutions, including bank, savings and loan offices, finance companies, credit unions and related services  |            | P         |
| 47. Refrigerator, furnace, water heater, and other household appliance sales including repair, provided repair services are incidental to retail sales.  |            | P         |
| 48. Laundries and dry cleaners   |            | P         |
| 49. Photocopying and related duplicating services not including printing, lithographing, engraving or such similar reproduction services   |            | P         |



| <i>Uses</i>  | <i>LI*</i> | <i>GC</i> |
|--|------------|-----------|
| 50. Pet and bird stores with incidental veterinary services  |            | P         |
| 51. Video stores, sales and rental   |            | P         |
| 52. Automobile and motorcycle sales, rental, including new and used car sales  |            | C         |
| 53. Bars without significant food service  |            | C         |
| 54. Building materials sales (no outside storage)  |            | C         |
| 55. Miniature golf   |            | C         |
| 56. Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, food and beverages (except alcoholic beverages), and small consumer items such as magazines, newspapers, etc.... |            | C         |

\*: Subject to compliance with the Environmental Performance Standards. Note that as defined by State law, any business using significant quantities of hazardous materials requires a Conditional Use Permit.

◆: Existing dairy operations are exempt from the Environmental Performance Standards.

## Consultant Acknowledgements

David Gates

Sandy Gimbal

**David Gates & Associates**

*Land Planning, Urban Design, Landscape Architecture*

**George Nickelson**

*Traffic Engineering*

Rod Andrade

Steve Lichliter

**MacKay & Soms**

*Civil Engineering*

RESOLUTION 99-106

AMENDING THE URBAN MANAGEMENT PLAN  
OF THE CITY OF TRACY  
THE APPLICANTS ARE DOVER/PRIMA ASSOCIATES AND  
F. E. BLINCOE, JR.  
APPLICATIONS 7-98-GPA AND 8-98-GPA

WHEREAS, The City of Tracy Planning Commission held a public hearing on an amendment to the Tracy Urban Management Plan on February 10, 1999 recommending that the 10 acres located on the southwest corner of Grant Line/Paradise Roads and the 10 acres located on the southwest corner of Pescadero Avenue and Paradise Road be redesignated from Commercial to Industrial, and

WHEREAS, The City Council of the City of Tracy held public hearing on April 6, 1999 on said amendment, and

WHEREAS, The City Council of the City of Tracy found said amendment consistent with the Tracy Urban Management Plan and Master Environmental Impact Report and the Environmental Impact Report for the Northeast Industrial Plan certified by the City Council on May 8, 1996;

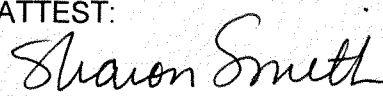
NOW, THEREFORE, BE IT RESOLVED, That the City Council of the City of Tracy does hereby approves the General Plan Amendment Applications 7-98-GPA and 8-98-GPA, by redesignating the 10 acre site located on the southwest corner of Grant Line and Paradise Roads and the 10 acre site located on the southwest corner of Pescadero Avenue and Paradise Road from Commercial to Industrial.

\*\*\*\*\*

The foregoing Resolution 99-106 was adopted by the Tracy City Council on the 6<sup>th</sup> day of April, 1999, by the following vote:

|          |                  |  |
|----------|------------------|--|
| AYES:    | COUNCIL MEMBERS: | IVES, MATTHEWS, TOLBERT, TUCKER, BILBREY |
| NOES:    | COUNCIL MEMBERS: | NONE                                     |
| ABSENT:  | COUNCIL MEMBERS: | NONE                                     |
| ABSTAIN: | COUNCIL MEMBERS: | NONE                                     |

  
\_\_\_\_\_  
Mayor

ATTEST:  
  
\_\_\_\_\_  
City Clerk

RESOLUTION 99-107

A RESOLUTION OF THE CITY COUNCIL  
OF THE CITY OF TRACY  
AMENDING THE NORTHEAST INDUSTRIAL CONCEPT  
DEVELOPMENT PLAN  
PROJECT CHARACTERISTICS, PROJECT OBJECTIVES

WHEREAS, The City Council of the City of Tracy adopted the Northeast Industrial Concept Development Plan on May 8, 1996, and

WHEREAS, The City of Tracy Planning Commission held a public hearing on an amendment to the Northeast Industrial Concept Development Plan on February 10, 1996, and

WHEREAS, The Planning Commission found said amendment consistent with the Tracy Urban Management Plan, and

WHEREAS, The City Council held a public hearing on the amendment to the Northeast Industrial Concept Development Plan on April 6, 1999;

NOW, THEREFORE, BE IT RESOLVED, That the City Council of the City Tracy does hereby approve the amendment to the Northeast Industrial Concept Development Plan as follows:

PROJECT CHARACTERISTICS

Project Objectives

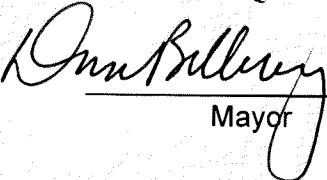
Land designated Industrial with zoning of Planned Unit Development (PUD), would permit commercial development without the site being redesignated Commercial if:

1. The site is a minimum of one acre and located on the northwest or southwest corners of Pescadero Avenue/Paradise Road or the northwest or southwest corners of Grant Line/Paradise Roads.
2. The site has City Engineer-approved access from at least two streets.
3. The proposal is for a convenience retail or general commercial service use to the industrial area, such as restaurants, hotels or convenience stores and service stations.

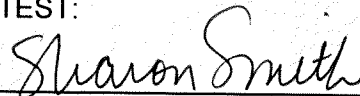
\*\*\*\*\*

The foregoing Resolution 99-107 was adopted by the Tracy City Council on the 6<sup>th</sup> day of April, 1999, by the following vote:

|          |                  |  |
|----------|------------------|--|
| AYES:    | COUNCIL MEMBERS: | IVES, MATTHEWS, TOLBERT, TUCKER, BILBREY |
| NOES:    | COUNCIL MEMBERS: | NONE                                     |
| ABSENT:  | COUNCIL MEMBERS: | NONE                                     |
| ABSTAIN: | COUNCIL MEMBERS: | NONE                                     |

  
\_\_\_\_\_  
Mayor

ATTEST:

  
\_\_\_\_\_  
City Clerk

RESOLUTION 2005-091

APPROVING THE PRELIMINARY AND FINAL DEVELOPMENT PLAN AND  
MINOR AMENDMENT TO THE NORTHEAST INDUSTRIAL CONCEPT DEVELOPMENT PLAN  
FOR A 390,000 SQUARE FOOT INDUSTRIAL BUILDING  
LOCATED ON A 17.93-ACRE PARCEL AT THE SOUTHEAST CORNER OF CHABOT COURT  
AND GRANT LINE ROAD; ASSESSOR'S PARCEL NUMBER 250-280-01 THROUGH 05  
APPLICATION NUMBER 45-04-D

WHEREAS, The subject property was annexed to the City of Tracy in 1996, received a zoning designation of Planned Unit Development, is designated Light Industrial in the Northeast Industrial Concept Development Plan, and is consistent with the General Plan designation of Industrial, and

WHEREAS, Trammell Crow Company, on behalf of M.I.R.E.F. Chabot Tracy, LLC, submitted an application for a Planned Unit Development Preliminary and Final Development Plan review (Application Number 45-04-D) and a minor amendment to the Northeast Industrial Concept Development Plan for a 390,000 square foot multi-tenant industrial building on December 6, 2004, and

WHEREAS, The subject property is located within the Northeast Industrial Concept Development Plan area, with a land use designation of Light Industrial, within which industrial land uses are permitted, and

WHEREAS, The Planning Commission conducted a public hearing to review and consider the application on March 9, 2005, and recommended, by adoption of a resolution, that the City Council approve the PUD Preliminary and Final Development Plan and minor Concept Development Plan amendment for a 390,000 square foot multi-tenant industrial building, and

WHEREAS, The City Council held a public hearing to discuss the application on April 5, 2005;

NOW, THEREFORE BE IT RESOLVED, That the City Council does hereby approve the PUD Preliminary and Final Development Plan and minor amendment to the NEI Concept Development Plan for a 390,000 square foot multi-tenant industrial building, Application No. 45-04-D, subject to the conditions contained in Exhibits 1 and 2 to this Resolution, and based on the following findings:

1. The establishment, maintenance, and operation of the proposed use and associated structure is compatible with the land use, design, and operational characteristics of the neighboring properties. It will not, under the circumstances of the particular case or as conditioned, be injurious or detrimental to the health, safety, or general welfare of persons or property in the vicinity of the proposed use and its associated structures, or to the general welfare of the City because the project is consistent with the land use, design, and other elements of the Northeast Industrial Concept Development Plan, the City of Tracy General Plan, and applicable requirements of Chapter 10.08 of the Tracy Municipal Code, including, but not limited to, Article 26, Off-Street Parking Requirements, and Article 30, Development Review.
2. The project will not adversely affect or impair the benefits of occupancy, most appropriate development, property value stability, or the desirability of property in the vicinity because the architectural elements of the project as designed and conditioned, are a quality addition to the

vacant parcel, and will not adversely visually impair the benefits of the properties in the vicinity as the project includes greater setbacks than the required minimum, vertical and horizontal variation in the building faces, and significant landscape improvements both adjacent to the building and the public rights-of-way.

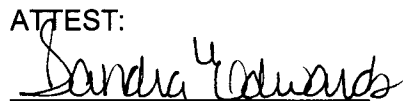
- 3. The project, as designed and conditioned, will not cause any significant environmental impact, because it is consistent with the Northeast Industrial Concept Development Plan and its Environmental Impact Report as adopted by the City Council in 1996. The project is consistent with the land use, design, and other elements of the Northeast Industrial Areas Concept Development Plan, the City of Tracy General Plan, and applicable requirements of the Tracy Municipal Code.

\*\*\*\*\*

The foregoing Resolution 2005-091 was adopted by the Tracy City Council on the 5<sup>th</sup> day of April, 2005, by the following vote:

|          |                  |  |
|----------|------------------|--|
| AYES:    | COUNCIL MEMBERS: | IVES, SUNDBERG, TOLBERT, TUCKER, BILBREY |
| NOES:    | COUNCIL MEMBERS: | NONE                                     |
| ABSENT:  | COUNCIL MEMBERS: | NONE                                     |
| ABSTAIN: | COUNCIL MEMBERS: | NONE                                     |

  
MAYOR

ATTEST:  
  
CITY CLERK

RESOLUTION 2008-046

APPROVING THE CONCEPT DEVELOPMENT PLAN AMENDMENTS FOR MAXIMUM BUILDING HEIGHT THROUGHOUT THE NEI CONCEPT DEVELOPMENT PLAN AREA AND TO ALLOW FOR A 15,000 SQUARE FOOT RETAIL AREA WITHIN AN INDUSTRIAL WAREHOUSE LOCATED ON A 160.34-ACRE SITE, LOCATED AT 1941 N. CHRISMAN ROAD - ASSESSOR'S PARCEL NUMBER 250-020-13  
APPLICATION NUMBER 1-08-SPA

WHEREAS, The Northeast Industrial Areas Concept Development Plan was approved by the City Council in 1996, and established the goals and standards for development within the area, and

WHEREAS, HPA, on behalf of Prologis Development Services, submitted an application for a Planned Unit Development Preliminary and Final Development Plan review and Concept Development Plan amendments for their project site (Application Numbers 6-06-D and 1-08-SPA) for three industrial buildings totaling 2,812,833 square feet, including 15,000 square feet of retail space on May 2, 2006, and

WHEREAS, Amendments to the NEI Concept Development Plan are required in order to approve the above project, including increasing the maximum height requirement for industrial buildings from 40 feet to 46 feet, and allowing retail development as an accessory use to the warehouse use on the site, and

WHEREAS, The Planning Commission conducted a public hearing to review and consider the applications on February 27, 2008 and recommended, by the adoption of a resolution that the City Council approve the proposed Concept Development Plan amendments;

NOW, THEREFORE BE IT RESOLVED, That the City Council does hereby approve the Concept Development Plan Amendment increasing the maximum building height from 40 feet to 46 feet throughout the NEI Concept Development Plan and allowing 15,000 square feet of retail space to be developed within the warehouse building on Assessor's Parcel Number 250-020-13, Application Number 1-08-SPA, based on the following findings:

1. The increase in maximum building height will not adversely affect or impair any properties in the vicinity of the NEI Concept Development Plan lands because there will not be conflict with large buildings in proximity to smaller scale developments, such as residential neighborhoods. The minor amendment to the NEI Concept Development Plan to revise the allowable building height to 46 feet will add to the aesthetic characteristics of the buildings by allowing for screening of roof equipment while keeping the necessary interior height for functionality.
2. The addition of a 15,000 square foot retail facility on Assessor's Parcel Number 250-020-13 will further the goals of the General Plan because it is consistent with the General Plan's Industrial land use designation in its allowing ancillary uses such as consumer services and retail within industrial areas.
3. The Concept Development Plan amendments will not cause any significant environmental impact, because they are consistent with the Northeast Industrial Concept Development Plan and its Environmental Impact Report as adopted by the City Council in 1996. The amendments are consistent with the land use, design, and other elements of the Northeast

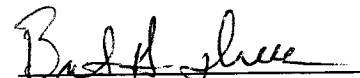


Industrial Areas Concept Development Plan, the City of Tracy General Plan, and applicable requirements of the Tracy Municipal Code.


\*\*\*\*\*

The foregoing Resolution 2008-046 was adopted by the City Council on the 18<sup>th</sup> day of March, 2008, by the following vote:

AYES: COUNCIL MEMBERS: ABERCROMBIE, SUNDBERG, TOLBERT, TUCKER, IVES  
NOES: COUNCIL MEMBERS: NONE  
ABSENT: COUNCIL MEMBERS: NONE  
ABSTAIN: COUNCIL MEMBERS: NONE

  
Mayor

ATTEST:

  
City Clerk

invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this chapter. The Council hereby declares that it would have adopted this chapter and each section, subsection, sentence, clause, phrase, or portion thereof, irrespective of the fact that any one or more sections, subsections, clauses, phrases, or portions be declared invalid or unconstitutional. (Prior code § 10-5.06)

## Chapter 10.20

### SPECIFIC PLANS\*

#### Sections:

|                  |   |
|------------------|---|
| <b>10.20.010</b> | <b>Authority for specific plans.</b>      |
| <b>10.20.020</b> | <b>Purpose.</b>                           |
| <b>10.20.030</b> | <b>Applicability.</b>                     |
| <b>10.20.040</b> | <b>Initiation procedures.</b>             |
| <b>10.20.050</b> | <b>Content of specific plan.</b>          |
| <b>10.20.060</b> | <b>Adoption and amendment procedures.</b> |
| <b>10.20.070</b> | <b>Environmental review.</b>              |
| <b>10.20.080</b> | <b>Fees.</b>                              |

#### **10.20.010 Authority for specific plans.**

Under the California Planning and Zoning Law, the City Council is authorized to prepare, adopt and implement a specific plan for any area covered by the City of Tracy General Plan. (Gov't. Code § 65450 and following.) This includes areas within the City boundaries, the City's sphere of influence, or the General Plan planning area.

(Ord. No. 1126, § 1, 11-18-2008)

#### **10.20.020 Purpose.**

The intent and purpose of this chapter is to establish uniform procedures for the consideration, adoption and implementation of specific plans. The purpose of the specific plan process is to provide an application tool for use in implementing the City of Tracy General Plan ("General Plan") on an area-specific basis. A specific plan is intended to serve as a policy and regulatory document, including policy direction and project development concepts which are consistent with the General Plan, the zoning ordinance and development standards.

(Ord. No. 1126, § 1, 11-18-2008)

#### **10.20.030 Applicability.**

(a) Consistency with existing plans. A specific plan shall be consistent with the General Plan. (Gov't. Code § 65454.) If there is an inconsistency between a specific plan and comparable regulations of the zoning ordinance, infrastructure master plan or development standards, the standards and regulations of the specific plan shall prevail.

Property annexed to the City within a specific plan area shall be zoned in conformance with the specific plan effective upon the effective date of annexation.

\*Editor's note—Ord. No. 1126, § 1, adopted Nov. 18, 2008, repealed the former ch. 10.20, §§ 10.20.010—10.20.200, and enacted a new ch. 10.20 as set out herein. The former ch. 10.20 pertained to similar subject matter and derived from Prior Code §§ 10-6.101—10-6.107, 10-6.201—10-6.207, 10-6.301—10-6.306.

(b) *Consistency; entitlements.* No building or grading permit, conditional use permit, variance, tentative map, final or parcel map, or any other land use entitlement shall be granted for any parcel covered by a specific plan unless the entitlement is consistent with the specific plan. (Government Code § 65455)

No public works project may be approved and no zoning ordinance adopted or amended unless consistent with the specific plan. (Government Code § 65455) (Ord. No. 1126, § 1, 11-18-2008)

#### 10.20.040 Initiation procedures.

(a) *General.* The proposal to prepare a specific plan may be initiated either by the City or by another applicant.

(b) *City-initiated specific plan.* A City-initiated proposal shall be submitted to the City Council for consideration. A written staff report shall include:

- (1) A description of the proposed project;
- (2) The reasons for the use of the specific plan process;
- (3) A vicinity map, drawn to scale, showing the proposed specific plan area and areas within one mile of the property;
- (4) A description of the current use of the property, and the addresses and assessor's parcel numbers for the properties, listed from the latest assessor's roll;
- (5) The planned land use designations for the properties shown on a map;
- (6) Proposed amendments to other, existing plans or proposals to amend zoning regulations;
- (7) A statement of how the specific plan implements the general plan and any anticipated general plan amendments; and
- (8) Any additional information required by the Director of Development and Engineering Services.

If the City Council agrees with the proposal or a modification, it shall adopt a resolution of intention to adopt a specific plan and related environmental review.

(c) *Application by another party.*

(1) *Complete application.* A party wishing the City to adopt a specific plan shall first submit a complete application to the Development and Engineering Services Department and pay an application fee as established by the City Council under subsection 10.20.080(a). The applicant is not required to own or otherwise control a majority of the properties within the proposed specific plan area. The application shall include a

City application form with accompanying diagrams and other pertinent information, including the information set forth in subsection (b) above.

(2) *Meeting with DES.* Within thirty (30) days of receipt of a complete application, the Development and Engineering Services Department shall schedule a meeting with the applicant to review the application. The department may propose modifications to the specific plan boundaries and scope in order to facilitate comprehensive planning. Due to the significant role an adopted specific plan plays in the implementation of the general plan, a project applicant should seek guidance from the Development and Engineering Services Department before submitting an application for a specific plan.

(Ord. No. 1126, § 1, 11-18-2008; Ord. No. 1144, § 2, 3-16-2010)

#### 10.20.050 Content of specific plan.

A specific plan, or a proposed specific plan amendment as applicable, shall include text and diagrams which specify all of the following in detail, and may also address other subjects (Government Code §§ 65451 and 65452):

- (a) The distribution, location and extent of the uses of land, including open space, within the area covered by the plan;
- (b) The proposed distribution, location and extent and intensity of major components of:
  - (1) Transportation (public and private);
  - (2) Wastewater (conveyance, treatment and discharge);
  - (3) Water (supply, treatment and transmission);
  - (4) Storm drainage (collection, storage, treatment and disposal);
  - (5) Public buildings and facilities (including City buildings, libraries, public safety facilities, parks and recreation); and

(6) Other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;

(c) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;

(d) A program of implementation measures including regulations, programs and public works projects; and

(e) Financing measures necessary to carry out the plan. (See Tracy Municipal Code subsections 10.20.060(b)(2) and (3) below.) (Ord. No. 1126, § 1, 11-18-2008; Ord. No. 1137, § 1, 2009)

#### 10.20.060 Adoption and amendment procedures.

(a) *General.* A specific plan shall be prepared, adopted and amended in the same manner as a general plan, except that a specific plan may be amended as often as necessary. (Government Code §§ 65453 and 65350 and following.) A specific plan amendment is not subject to the initiation procedures set forth in section 10.20.040, except at the discretion of the Director based on the size and complexity of the proposed amendment.

(b) *Findings.* Before the Planning Commission recommends approval, or the City Council approves it, a specific plan (or amendment) must:

(1) Be consistent with the general plan;

(2) Be consistent with the City-approved infrastructure master plans or adequately show alternative infrastructure needed to support the land uses described in the specific plan; and

(3) Contain a requirement to ensure that each property owner within the specific plan has paid or is required to pay its fair share of:

(A) The cost of the infrastructure master plans; and

(B) The cost of infrastructure required by the infrastructure master plans.

This subsection (b)(3) may be satisfied by an approved finance and implementation plan (FIP) and adoption of a development impact fee for each component of infrastructure, City Council approval of a Community Facilities District (CFD) under Government Code § 53311 and following, or other equally detailed plan and implementation.

(Ord. No. 1126, § 1, 11-18-2008; Ord. No. 1137, § 2, 2009)

#### 10.20.070 Environmental review.

A specific plan is subject to the California Environmental Quality Act (CEQA; Pub. Res. Code § 21000 and following. See also Gov't. Code § 65457.)

(Ord. No. 1126, § 1, 11-18-2008)

#### 10.20.080 Fees.

(a) *Application and processing.* An applicant other than the City shall pay the application and processing fees established by City Council resolution. This includes processing fees, through a cost recovery agreement, intended to cover the costs under subsection (b).

(b) *Specific plan fee.* The City Council may establish a specific plan fee for applicants seeking a land use entitlement within the area of a proposed or an already-adopted specific plan. (Government Code § 65456.) A specific plan fee shall only be established to reimburse the City for costs it incurs for preparation, adoption and administration of the specific plan (whether or not such costs are otherwise covered by a cost recovery agreement with a developer). It does not apply to a developer's own costs or its consultants' costs for a specific plan.

The purpose of the fee is to defray the cost of preparation, adoption and administration of the specific plan and to reimburse the City or other paying party for the costs of preparing the specific plan, including costs of consultants and environmental review. The fees charged shall be a prorated amount in accordance with the applicant's relative benefit derived from the specific plan. In the aggregate, the fees shall defray, but not exceed, the cost of preparation, administration and adoption of the specific plan.

(c) *Infrastructure master plan costs.* Because infrastructure master plans are a critical element of a workable specific plan, an applicant for a specific plan shall pay its fair share of the cost of the infrastructure master plans. The amount of the infrastructure master plan costs shall be established through one or more infrastructure master plan processing agreements and payment is a condition of proceeding with the specific plan application after the City Council direction to proceed. (The infrastructure master plan processing agreements are distinguished from the cost of implementing the infrastructure master plans through impact fees or other measures.)

(Ord. No. 1126, § 1, 11-18-2008; Ord. No. 1137, § 2, 2009; Ord. No. 1144, § 3, 3-16-2010)

~~Northeast Industrial~~  
~~Concept Development Plan~~  
City of Tracy  
**NORTHEAST INDUSTRIAL**  
**SPECIFIC PLAN**

July 17, 2012

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| <del>Background Traffic Growth / Traffic Operations</del>                                 | <del>14</del> |
| <del>Traffic Increases Due to the Project</del>   | <del>14</del> |
| <del>Traffic Generation</del>   | <del>14</del> |
| <del>Circulation Plan</del>   | <del>15</del> |
| <del>Proposed Internal Street Network</del>   | <del>15</del> |
| <del>Parcel Access</del>  | <del>15</del> |
| <del>Rail Access</del>  | <del>15</del> |
| <del>Truck Routes</del>   | <del>17</del> |
| <del>Relationship Between the Proposed Circulation Plan and the Roadway Master Plan</del> | <del>17</del> |
| <del>Phasing of Roadway Sections</del>  | <del>20</del> |
| PUBLIC SERVICES AND INFRASTRUCTURE  | <u>2017</u>   |
| Wastewater Collection and Disposal  | <u>2017</u>   |
| <del>City Master Plan</del>   | <del>20</del> |
| <del>Sanitary Sewer Serviceability Analysis</del>   | <del>22</del> |
| Storm Drainage  | <u>2419</u>   |
| <del>City Master Plan</del>   | <del>24</del> |
| <del>Storm Drain Serviceability Analysis</del>  | <del>24</del> |
| Water Supply and Distribution   | <u>2621</u>   |

|  |              |
|--|--------------|
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| <u>City Master Plan Distribution Network</u>             | <u>26</u>    |
| <u>Water System Serviceability Analysis</u>              | <u>26</u>    |
| Police Protection  | <u>2621</u>  |
| Fire Protection  | <u>2621</u>  |
| Other Utilities  | <u>2621</u>  |
| <u>PHASING AND FINANCING</u>                             | <u>29</u>    |
| <u>Phasing</u>   | <u>29</u>    |
| <u>Financing and Fiscal</u>                              | <u>29</u>    |
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| Recommended Trees for Major Streets                      | <u>3023</u>  |
| Street Lighting  | <u>3124</u>  |
| Building FAR and Height                                  | <u>3124</u>  |
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| Signs  | <u>3732</u>  |
| Landscaping  | <u>3933</u>  |
| Screening and Storage                                    | <u>4034</u>  |
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| Environmental Performance Standards                      | <u>4234</u>  |
| Use Restrictions   | <u>4234</u>  |
| Hazardous Wastes and Water Pollutants                    | <u>4235</u>  |
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Table 18: Permitted and Conditional Industrial and Commercial Uses

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## INTRODUCTION

The ~~Conceptual Development Plan for the~~ 870 acre Northeast Industrial Area ~~Specific Plan~~ aims to develop a well-planned industrial zone that will attract businesses to Tracy, and provide local employment opportunities. The ~~Specific~~ Plan anticipates a mixture of industrial uses, including rail-dependent industries and "flex-tech" light industrial.

## PLANNING AREA LOCATION

### Location

The Northeast Industrial planning area lies along the northeast boundary of the City of Tracy. The area is generally bounded to the north by 1-205, to the south by the Southern Pacific Railroad tracks, to the east by Banta Road, and the west by MacArthur Drive. Grant Line Road bisects the area.

### Surrounding Land Uses

Presently, properties along MacArthur Drive directly west of the site are developed with industrial uses, such as the U.S. Cold Storage facility. North of the site are industrial and commercial uses, including the ~~factory outlet~~ ~~Tracy Outlets~~ stores at MacArthur Drive and Pescadero Avenue. The Yellow Freight Company is also located to the north, between the site and 1-205. Agricultural uses are found on lands to the east.

Lands directly to the north and west of the site are included in the ~~1991~~ 1-205 Corridor Specific Plan and the 1988 Industrial Areas Specific Plan. To the east is the ~~residential~~ Banta ~~Community Area of the county, as designated in the General Plan~~. The project site in the context of the existing industrial and commercial uses, and existing specific plan areas is shown in Figure 1.

### Land Ownership

The Northeast Industrial planning area is ~~currently~~ made up of ~~some 32-61~~ properties, with a variety of owners. Parcels range in size from small half acre home sites to large agricultural ~~and industrial~~ holdings. The assessor's parcel numbers and ownership of the area is shown in Figure 2.

## EXISTING PLANNING AREA CONDITIONS

### Site Features

Land uses within the Northeast Industrial Area are currently light industrial and agricultural, with a number of dairy operations and rural residences remaining. Few other significant site features are present. The topography is relatively level.

A Westside Irrigation District (WSID) supply or tailwater ditch crosses the area. One property along 1-205 is located just outside the FEMA 100 year flood line found in the vicinity of the I-205/Paradise Road overcrossing. ~~It is believed that theoretical flood depths on the adjacent parcel are only a few feet.~~

The existing conditions ~~and topography~~ of the planning area are shown on Figures 3 and 4. None of the parcels ~~appear to be~~ under Williamson Act contracts, ~~although further research should verify the Williamson status.~~



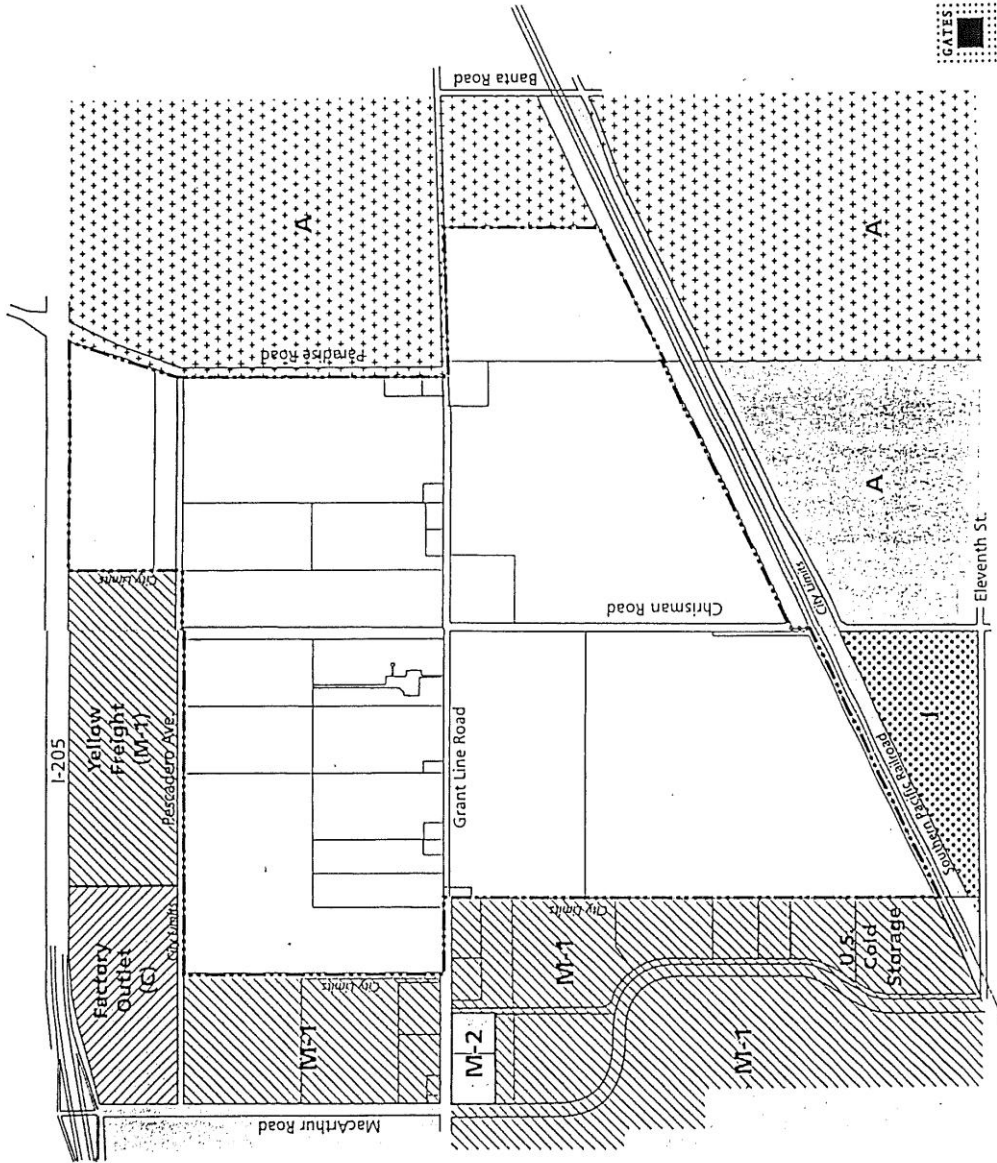


Figure 1

# SURROUNDING LAND USES

## LEGEND

- Tracy City Limits
- Agricultural Lands (County)
- Industrial Lands (County)
- Industrial Specific Plan
- I-205 Specific Plan
- Light Industrial Zoning (Tracy)
- Heavy Industrial Zoning (Tracy)
- Agricultural Lands (Tracy)
- Project Boundary



February 26, 1996

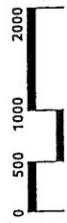




Figure 2

LAND OWNERS

| No. | APN       | OWNER'S NAME          | ACRES |
|-----|-----------|-----------------------|-------|
| 1   | 212662-12 | SIWA PARTNERS         | 42.42 |
| 2   | 212662-13 | DOROTHY PALLEY        | 9.78  |
| 3   | 212676-37 | VELMA PRINCE ET AL.   | 77.95 |
| 4   | 212676-26 | VELMA PRINCE AND SONS | 19.24 |
| 5   | 212676-27 | VELMA PRINCE ET AL.   | 19.24 |
| 6   | 212676-24 | SIWA ET AL.           | 5.54  |
| 7   | 212676-19 | MAMA SIWA ET AL.      | 71.86 |
| 8   | 212676-18 | MAMA SIWA ET AL.      | 19.55 |
| 9   | 212676-13 | FRMA ASSOCIATES       | 76.04 |
| 10  | 212676-12 | FRMA ASSOCIATES       | 76.04 |
| 11  | 212676-11 | HELENE MATTHEW        | 31.8  |
| 12  | 212676-29 | HELENE MATTHEW        | 61.8  |
| 13  | 212676-28 | F. SIWA               | 18.18 |
| 14  | 212676-15 | STANLEY KOBERTSON     | 9.71  |
| 15  | 212676-14 | WESLEY ENDEE          | 9.71  |
| 16  | 212676-13 | WESLEY ENDEE          | 9.71  |
| 17  | 212676-11 | AUGUST MARTIN         | 43.3  |
| 18  | 212676-17 | AUGUST MARTIN         | 23.27 |
| 19  | 212676-25 | AUGUST MARTIN         | 11.41 |
| 20  | 212676-24 | AUGUST MARTIN         | 11.41 |
| 21  | 212676-23 | MAMBI SIWA            | 11.8  |
| 22  | 212676-22 | MAMA SIWA ET AL.      | 17.55 |
| 23  | 212676-11 | FRANK ENDEE           | 43.7  |
| 24  | 212676-14 | WILLIAM ENDEE         | 6.96  |
| 25  | 212676-13 | WILLIAM ENDEE         | 6.96  |
| 26  | 212676-12 | WILLIAM ENDEE         | 6.96  |
| 27  | 212676-11 | WILLIAM ENDEE         | 6.96  |
| 28  | 212676-10 | WILLIAM ENDEE         | 6.96  |
| 29  | 212676-09 | WILLIAM ENDEE         | 6.96  |
| 30  | 212676-08 | WILLIAM ENDEE         | 6.96  |
| 31  | 212676-07 | WILLIAM ENDEE         | 6.96  |
| 32  | 212676-06 | WILLIAM ENDEE         | 6.96  |
| 33  | 212676-05 | WILLIAM ENDEE         | 6.96  |
| 34  | 212676-04 | WILLIAM ENDEE         | 6.96  |
| 35  | 212676-03 | WILLIAM ENDEE         | 6.96  |
| 36  | 212676-02 | WILLIAM ENDEE         | 6.96  |
| 37  | 212676-01 | WILLIAM ENDEE         | 6.96  |
| 38  | 212676-00 | WILLIAM ENDEE         | 6.96  |
| 39  | 212676-99 | WILLIAM ENDEE         | 6.96  |
| 40  | 212676-98 | WILLIAM ENDEE         | 6.96  |
| 41  | 212676-97 | WILLIAM ENDEE         | 6.96  |
| 42  | 212676-96 | WILLIAM ENDEE         | 6.96  |
| 43  | 212676-95 | WILLIAM ENDEE         | 6.96  |
| 44  | 212676-94 | WILLIAM ENDEE         | 6.96  |
| 45  | 212676-93 | WILLIAM ENDEE         | 6.96  |
| 46  | 212676-92 | WILLIAM ENDEE         | 6.96  |
| 47  | 212676-91 | WILLIAM ENDEE         | 6.96  |
| 48  | 212676-90 | WILLIAM ENDEE         | 6.96  |
| 49  | 212676-89 | WILLIAM ENDEE         | 6.96  |
| 50  | 212676-88 | WILLIAM ENDEE         | 6.96  |
| 51  | 212676-87 | WILLIAM ENDEE         | 6.96  |
| 52  | 212676-86 | WILLIAM ENDEE         | 6.96  |
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| 56  | 212676-82 | WILLIAM ENDEE         | 6.96  |
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| 58  | 212676-80 | WILLIAM ENDEE         | 6.96  |
| 59  | 212676-79 | WILLIAM ENDEE         | 6.96  |
| 60  | 212676-78 | WILLIAM ENDEE         | 6.96  |
| 61  | 212676-77 | WILLIAM ENDEE         | 6.96  |
| 62  | 212676-76 | WILLIAM ENDEE         | 6.96  |
| 63  | 212676-75 | WILLIAM ENDEE         | 6.96  |
| 64  | 212676-74 | WILLIAM ENDEE         | 6.96  |
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| 66  | 212676-72 | WILLIAM ENDEE         | 6.96  |
| 67  | 212676-71 | WILLIAM ENDEE         | 6.96  |
| 68  | 212676-70 | WILLIAM ENDEE         | 6.96  |
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| 71  | 212676-67 | WILLIAM ENDEE         | 6.96  |
| 72  | 212676-66 | WILLIAM ENDEE         | 6.96  |
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| 86  | 212676-52 | WILLIAM ENDEE         | 6.96  |
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| 90  | 212676-48 | WILLIAM ENDEE         | 6.96  |
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| 247 | 212674-90 | WILLIAM ENDEE         | 6.96  |
| 248 | 212674-89 | WILLIAM ENDEE         | 6.96  |
| 249 | 212674-88 | WILLIAM ENDEE         | 6.96  |
| 250 | 212674-87 | WILLIAM ENDEE         | 6.96  |
| 251 | 212674-86 | WILLIAM ENDEE         | 6.96  |
| 252 | 212674-85 | WILLIAM ENDEE         | 6.96  |
| 253 | 212674-84 | WILLIAM ENDEE         | 6.96  |
| 254 | 212674-83 | WILLIAM ENDEE         | 6.96  |
| 255 | 212674-82 | WILLIAM ENDEE         | 6.96  |
| 256 | 212674-81 | WILLIAM ENDEE         | 6.96  |
| 257 | 212674-80 | WILLIAM ENDEE         | 6.96  |
| 258 | 212674-79 | WILLIAM ENDEE         | 6.96  |
| 259 | 212674-78 | WILLIAM ENDEE         | 6.96  |
| 260 | 212674-77 | WILLIAM EN            |       |

### Existing Roadway Network

The Plan area is served by a number of existing roadways, some of which have been improved to urban standards and some which remain essentially rural in character, meet the circulation needs of the NEI area, and others that will be constructed or improved as traffic demand requires.

Regional access for the area is provided by Interstate 205 (I-205) to the north and Interstate 5 (I-5) to the east. I-205 is an east-west freeway which extends from I-5 west to I-580 with I-580 continuing west into the Bay Area. I-5 is a north-south freeway which extends throughout Central California. Currently, Plan area access to I-205 is via an interchange at MacArthur Drive. I-5 access is via the full access interchange at Kasson Road (an extension of Grant Line Road). I-5 access is also available (to/from the north only) via an interchange at Eleventh Street. The primary local roadways serving the area are MacArthur Drive, Pescadero Avenue, East and Grant Line Road, Chrisman Road and Paradise Road. MacArthur Drive is a north-south major arterial roadway serving the east side of Tracy. In the Plan area, MacArthur Drive has been improved to a four-lane roadway (with turn lanes) between I-205 and Eleventh Street. Pescadero Avenue is an east-west roadway which has been improved to four lanes (with turn lanes) from MacArthur easterly about 1/4 mile. East of this point, Pescadero Avenue narrows to a wide two-lane roadway along the Yellow Freight frontage (a distance of about 2/3 mile.) Between this point and Paradise Road, Pescadero Avenue is a narrow two-lane rural road. Grant Line Road is an improved four-lane arterial road from MacArthur Drive easterly about 1/4 mile (to the City limits). East of this point, Grant Line Road is a wide two-lane road extending to (and beyond) I-5.

In the Plan area, Chrisman Road and Paradise Road currently provide limited local access. Chrisman Road is a narrow two-lane rural road which extends from Grant Line Road south to the southern Pacific railroad tracks (although Chrisman Road does not cross the tracks). Paradise Road is a two-lane road extending from Grant Line Road north over I-205.

### Current Traffic Operations

Current volumes on the area's street network are relatively low. Traffic operations are very stable with area streets operating at Level of Service (LOS) 'C' or better.

## EXISTING UTILITY INFRASTRUCTURE

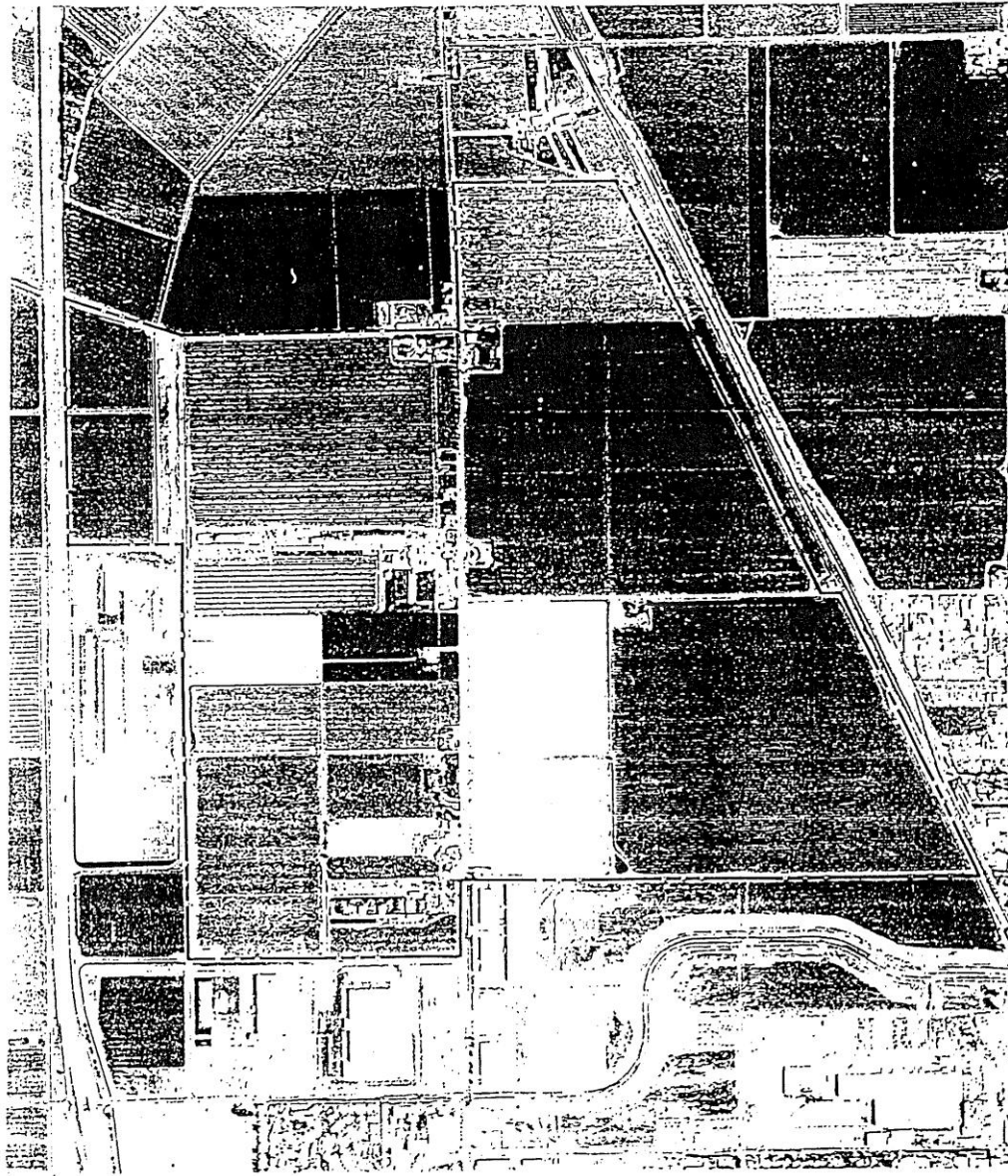
### Existing Wastewater Collection Facilities

As shown on Figure 9 (page 21) the East Side Trunk sewer line flows from south to north, and is located west of the subject site and just west of MacArthur Drive. Feeder lines join the East Side Trunk from the west and east.

There is a 12" diameter sanitary sewer on Grant Line Road stubbed to a point about 400' west of the site. There is also a sanitary sewer line on Pescadero Avenue which ranges in size from 18" to 8" diameter. The City of Tracy Public Works Department has indicated this line connects to the East Side Trunk, but has not been able to provide any information about it.

### Existing Storm Drainage Facilities

As shown on Figure 11 (page 25) the most significant existing drainage facility in the vicinity of the Northeast Industrial planning area is the East Side Channel, which flows to the north, west of the site. There is also a 48" diameter storm pipe in Grant Line Road and a pipeline in Pescadero Avenue with a diameter that varies from 15" to 24". These pipes both drain into the East Side Channel.



# Northeast Industrial

## Figure 3 AERIAL PHOTOGRAPH

source: Pacific Aerial Survey, 10/16/95

February 26, 1996






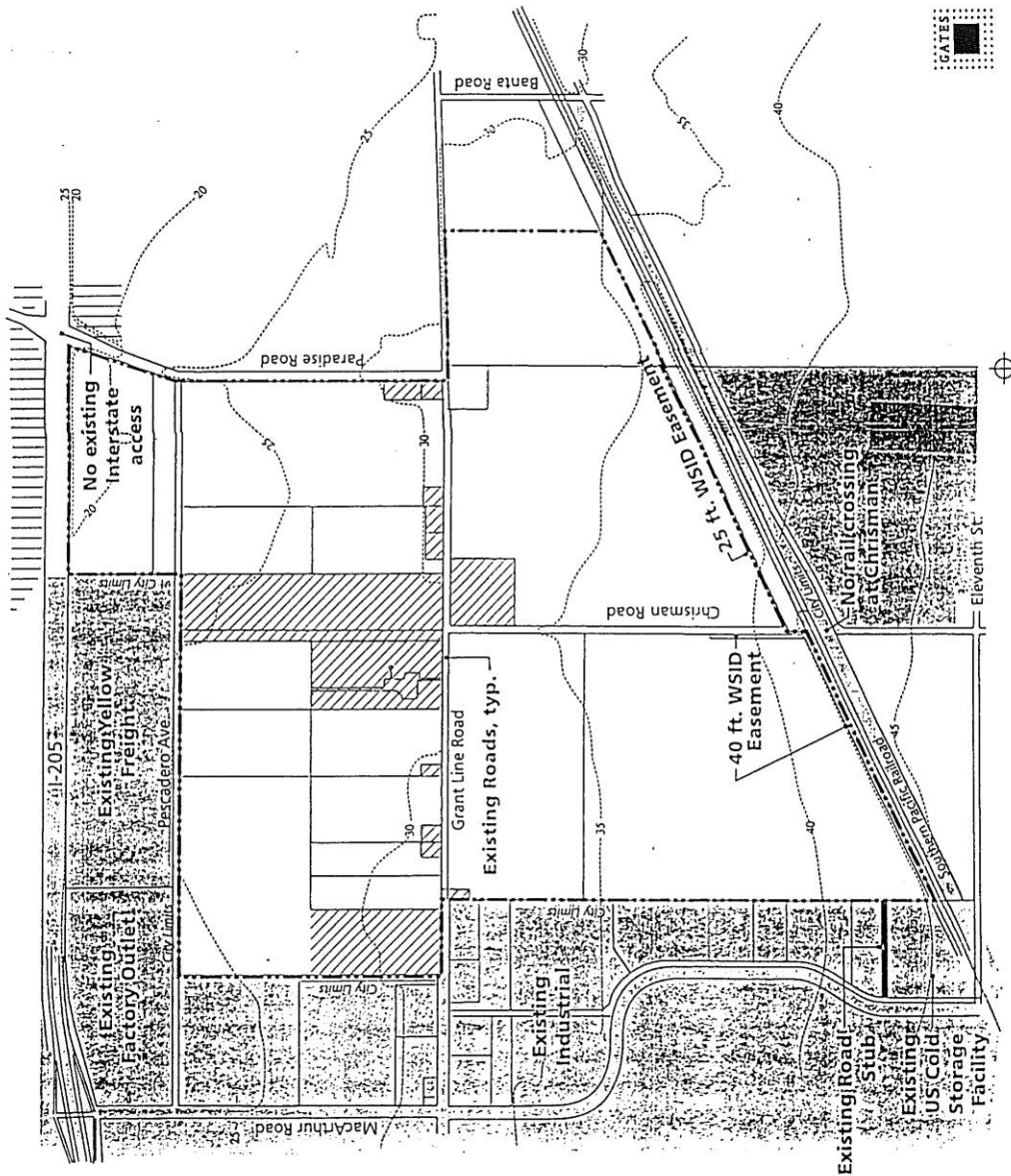
# Northeast Industrial

Figure 4

## EXISTING SITE CONDITIONS

### LEGEND

-  FEMA 100 Year Flood Zone
-  Existing Dairies and Homes
-  Tracy City Limits



source: (topography) USGS

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### **Existing Water Distribution Facilities**

As shown on Figure 12 (page 27), at this time a 12" to 18" diameter water main exists along Grant Line Road from MacArthur Drive to the west end of the site. A 16" diameter main currently is in place along Pescadero Avenue from MacArthur Drive to the west end of the most northerly portion of the site. There is also a 12" main in MacArthur Drive.

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## **PLANNING OVERVIEW**

This Conceptual Development Plan is submitted as the first step in the planned development process as defined under the appropriate provisions of the Tracy Municipal Code. The Planning steps are envisioned as follows:

**Task 1:** Concept Development Plan, Preliminary Development Plan, Preliminary Development Plan, PD pre-zoning, Finance Plan

The Concept Development Plan will be reviewed by City staff and the City's processing consultant. The City will prepare an EIR, and other documents necessary to complete the CEQA environmental review process. The Concept Development Plan will satisfy both the Concept Development Plan and Preliminary Development Plan requirements of Tracy's Planned Unit Development Ordinance. Once the plan is approved by the City, the area will be pre-zoned PD Planned Unit Development.

A financing Plan for area-wide public improvements will be submitted separately and prepared in consultation with the City.

**Task 2:** Annexation, Development Agreements

Following approval of the Concept Development Plan and receipt of the PD pre-zoning, the lands in the Northeast Industrial Area will be annexed to the City of Tracy and formally zoned PD. Development Agreements may be prepared for each of the properties, at each owner's discretion.

**Task 3:** Final Development Plans, Subdivision Maps

Each individual land owner will prepare subsequent Final Development Plans (FDP) for their parcels. Subdivision Maps may be submitted with the FDP. Parcelization may occur at any of these steps.

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## **RELATIONSHIP TO TRACY'S UMP-GENERAL PLAN**

### **UMP Land Use Designations**

The City of Tracy Urban Management Plan (UMP) designated the entire area for Industrial land uses, as shown in Figure 5. The UMP designated the planning area as part of the City Core Contiguous area, which encompasses the land areas adjacent to the existing urbanized areas of Tracy. In the City Core Contiguous area, new growth can directly tie into existing service and infrastructure system.

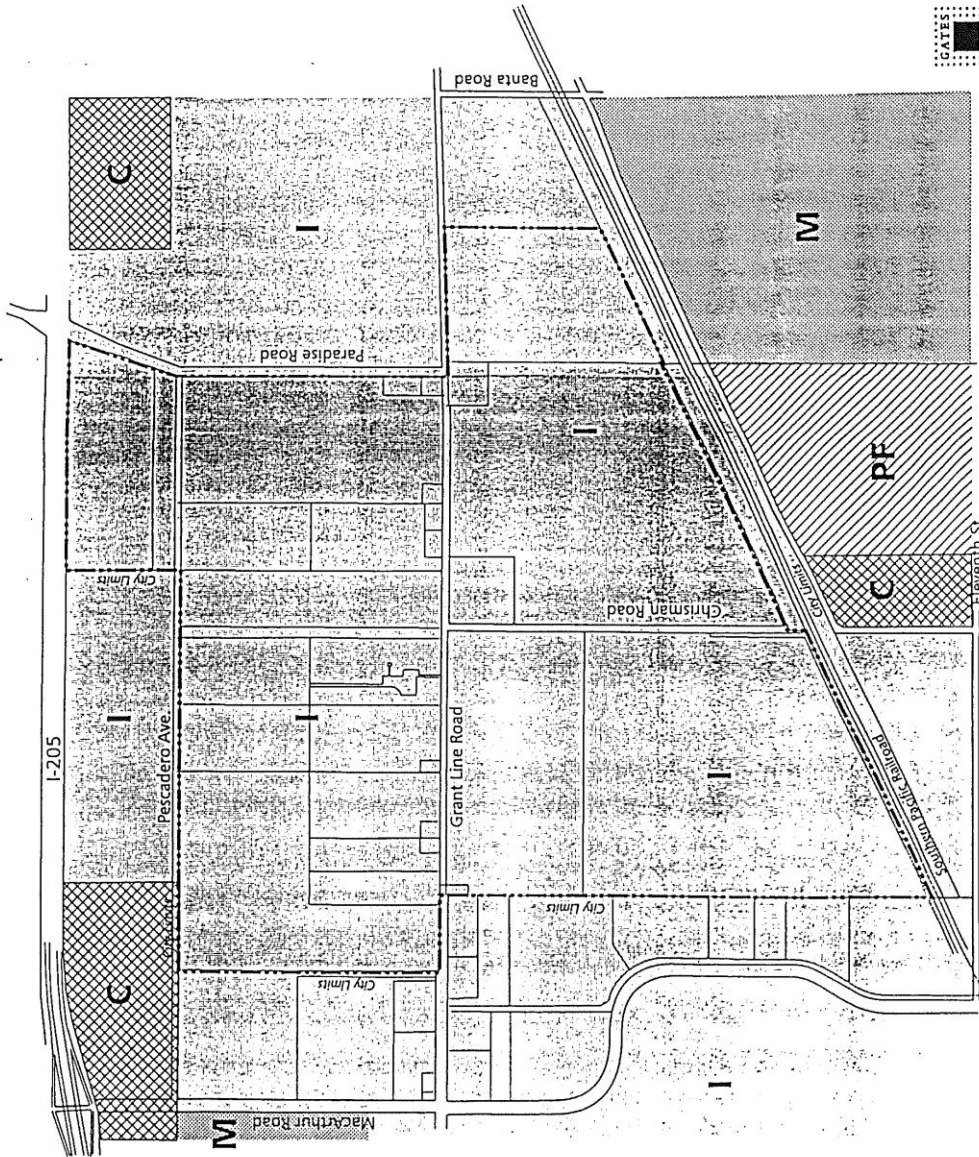
# Northeast Industrial

Figure 5

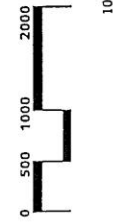
## UMP LAND USES

### LEGEND

- Commercial
- Industrial
- Public Facilities
- Medium Density Residential



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**General Plan Land Use Designations**

The City of Tracy General Plan designates the entire Northeast Industrial area for Industrial land uses. Specific uses allowed in the industrial category range from flex/office space to manufacturing to warehousing and distribution. Ancillary uses, such as restaurants, consumer services, and parks may be allowed to serve the daily needs of the workers.

**Tracy’s Growth Strategy**

In its UMP, the City of Tracy has embraced a “balanced growth” strategy, seeking to direct growth in an efficient, cost-effective manner, balancing land uses and appropriate use of the land with well-planned and utilized infrastructure. This strategy seeks to improve the jobs/housing balance and to encourage development of employment opportunities and capitalize on freeway interchanges with industrial, retail, and service-related development. An important component of the economic strategy behind this plan is to provide for a more than adequate amount of commercial and industrial land to keep land costs down in order to attract businesses to Tracy, thus providing local jobs for Tracy residents.

The northeastern sector of Tracy has been designated as one area in which the City’s industrial growth will occur. Adjacent to existing industrial development and with direct access to the I-205 freeway and rail transportation, the Northeast Industrial area is ideally situated to attract and support business without the need for major infrastructure expenditures. It is the logical next step for industrial expansion in Tracy.

**UMP General Plan Goals Furthered by the Northeast Industrial Area Development**

The Northeast Industrial Specific Plan Area Concept Development Plan (CDP) proposes development consistent with the UMP General Plan. It furthers many of the Goals and Policies stated in the UMP General Plan and it implements many of the Actions set forth in the UMP General Plan. Following is a summary of UMP General Plan Goals, Policies, and Actions to which the Northeast Industrial Area Specific Plan contributes.

**Land Use**

~~GOAL LU1: A balance between residential population, jobs, and ability to provide services.~~

~~Action LU 1.1.1: Industry will be generally targeted to the northeast, south and southwest of Tracy.~~

~~Policy LU 2.4: Provide flexibility in land use planning (including regulation of land use intensity, distribution, balance) to be responsive to market.~~

~~GOAL LU 6: A land use mix that provides employment opportunities for all who live in Tracy and wish to work here.~~

~~Action LU 6.1.2: Designate area around existing and planned industry for compatible land uses.~~

~~Policy LU 6.2: Encourage a diversity of industries.~~

~~Policy LU 6.3: Maintain Tracy’s competitiveness in attracting industries looking for relocate to the Central Valley.~~

~~GOAL LU 7: Land use patterns that minimize conflicts between neighboring uses and transportation corridors.~~



- ~~———— Policy LU 7.3: Locate compatible development near and along freeway corridors, and provide adequate environmental protection to less compatible uses.~~
  - ~~———— Policy LU7.4: Implement freeway uses which are compatible with the noise, air quality, and traffic impacts associated with freeways.~~
  - ~~———— Action LU 7.5.1: Locate employment-generating and regional commercial uses along major transportation corridors.~~
  - ~~———— Policy LU 7.7: Establish land uses along freight rail lines that are consistent and compatible with rail service and exposure.~~
- ~~GOAL LU8: Continue agriculture and resource extraction for as long as they can be conducted in an economically viable fashion.~~
- ~~GOAL LU 9: Maintain economic viability as a community.~~
- ~~———— Policy LU 9.2: Tracy is to remain a central urban service provider and become a retail and industrial center.~~

**Circulation**

- ~~GOAL CI 3: Minimize use of city streets by inter-regional commuter traffic and trucks.~~
- ~~———— Policy CI 3.4: Achieve direct movement of trucks from major generators to the freeways and other inter-regional routes via streets planned and designed for heavy vehicles.~~
  - ~~———— Action CI 3.4.4: Limit new truck traffic-generating uses to locations along the freeways, expressways and truck routes.~~
- ~~GOAL CI 8: Efficient movement of truck traffic through and around the city with minimum impacts on residential and commercial areas.~~

**Objective LU-1.1 Establish a clearly defined urban form and city structure.**

Policy P1: New development and redevelopment in existing areas shall be organized as a series of residential Neighborhoods, Employment Areas, Corridors, Village Centers, the Downtown and the I-205 Regional Commercial Area.

- Employment Areas are the job-centers of the city and include office districts, retail centers and industrial areas.

**Objective LU-2.3 Expand the City’s industrial base.**

Policy P1: The Northeast Industrial Area should contain a mix of heavy industrial, light industrial, warehouse, and distribution users to maximize rail and highway access on large parcels of land. The

Northeast Industrial Area should also contain commercial uses and services to meet the daily needs of workers.

**Objective LU-6.2 Ensure land use patterns that minimize conflicts between transportation corridors and neighboring uses.**

Policy P1: Uses that are compatible with the noise, air quality and traffic impacts associated with freeways, such as auto-oriented commercial and industrial uses, should be located near and along freeway corridors whenever possible.

**Economic Development**

**Objective ED-4.1 Ensure an adequate, balanced supply of all land uses for future economic development.**

Action A1: Monitor current and future land supply needs for industrial, office and retail growth.

**Objective ED-6.3 Promote expansion in the Northeast Industrial Area.**

Policy P1: The City shall encourage and facilitate the development and buildout of the entire 870-acre Northeast Industrial Area.

Policy P2: The City shall direct business attraction efforts to manufacturing uses, rather than warehouse distribution facilities, due to their higher employment densities.

Policy P3: Developers should consider flexible facility design and construction types that can accommodate future manufacturing uses with higher employment densities.

Policy P4: The City should support efforts to attract private developers and equity investors to participate in the development of the area.

**Community Character**

**Objective CC-11.2 Encourage attractive design in Employment Areas.**

Policy P1: Development in Employment Areas should adhere to high-quality design standards.

Policy P4: Building setbacks for office buildings or office portions of industrial buildings should be minimized to ensure that buildings define the edges of the street.

Policy P6: Loading facilities in Employment Areas should be screened from view from public streets to the extent possible.

Policy P9: Fencing visible from the public right-of-way shall be visually appealing when used in industrial and commercial developments.

**Objective CC-11.3 Minimize the impact of parking on the pedestrian environment in Employment Areas.**

Policy P1: The impact of parking in Employment Areas on the pedestrian environment should be minimized with attractive landscaping.

Policy P2: Parking lots should be set back from the street with a landscaped buffer wherever possible.

**Circulation**

**Objective CIR-1.4 Protect residential areas from commercial truck traffic.**

Policy P1: Significant new truck traffic generating uses shall be limited to locations along designated truck routes, in industrial areas or within ¼-mile of freeways.

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## PROJECT CHARACTERISTICS

### Project Objectives

The objectives of the project are:

- To develop the Northeast Industrial Area as a high-quality industrial and commercial site of significant benefit to the City of Tracy and the nearby region.
- To develop a well-planned site that will attract businesses to Tracy, providing local employment opportunities.
- To develop the Northeast Industrial Area for primarily mixed industrial uses, including rail-dependent industries.
- To minimize project-related impacts to Tracy's transportation network.
- To provide a flexible phasing program that allows market forces to dictate reasonable growth increments, while ensuring that agricultural properties are allowed to remain until ready to develop.
- To create a project consistent with the goals of the General Plan.
- To integrate the Northeast Industrial Area into the development pattern of the City of Tracy.
- To integrate mitigation for environmental impacts into the design of the project.

### Land Use Designations

The Northeast Industrial Area will consist primarily of light industrial land uses. In addition, general commercial land uses are planned at major intersections along Pescadero Avenue and Grant Line Road. Figure 5 shows the configuration of the land uses in the planning area.

The development prototypes included in the Design Guidelines indicate possible site planning scenarios for environmental impact analysis.

### Light Industrial Land Uses (LI)

Light Industrial land uses cover approximately 800 acres of the planning area. Assuming a Floor Area Ratio (FAR) of 0.5, approximately 17.5 million square feet of light industrial, warehouse and manufacturing facilities would be provided. Light industrial land uses would be compatible with existing industrial land uses to the west and north, as well as with freeway noise, and rail noise and vibration.

Several types of light industrial land uses are appropriate in the Northeast Industrial Area. It is anticipated that warehousing and distribution businesses with low employee densities will be the predominant development type. This development pattern is similar to those that have located in Tracy in recent years. The southern portion of the Planning Area is appropriate to uses that require rail access. ~~It is assumed that about 80% of the light industrial lands will be developed with warehouse and distribution similar to the type of development existing in Tracy.~~

The City of Tracy is also interested in attracting higher employee density businesses to the area. It is anticipated that there may be a future demand for a "Flex-Tech" development that would accommodate research & development businesses and call centers. ~~For purposes of analysis, it is assumed that about 20% of the light industrial lands will be developed with such higher employee density businesses.~~

The light industrial zone may also be appropriate for service commercial businesses with little pedestrian traffic, that are not necessarily compatible with general commercial land uses, such as automotive supply and plumbing

stores. Commercial development may be permitted on sites designated Light Industrial without the site being redesignated General Commercial if:

1. The site is a minimum of one acre and located on the northwest or southwest corners of Pescadero Avenue and Paradise Road or the northwest or southwest corners of Grant Line Road and Paradise Road.
2. The site has City Engineer-approved access from at least two streets.
3. The proposal is for a convenience retail or general commercial service use to the industrial area, such as restaurants, hotels, or convenience stores and service stations.
4. 15,000 square feet of retail within an industrial warehouse located on the southwest corner of Chrisman Road and Paradise Road.

A variety of parcel sizes are foreseeable, probably ranging from 20-5 to 50 acres, depending upon the type of industrial user attracted to the area. Since market forces will dictate that eventual parcel size, the CDP-Specific Plan does not reflect an exact parcelization plan.

### **Existing Agricultural and Residential Uses**

The parcels containing existing dairy operations and residences, shown on the Land Use Map, that may remain for a number of years until ready to develop. The planned light industrial land uses will be compatible with these properties. (The agricultural lands are subject to the City's Right to Farm Ordinance, discussed below)

### **General Commercial Land Uses (GC)**

Three parcels of general commercial, totaling 45.5 acres are provided within the planning area. These commercial sites are intended to expand upon the existing commercial uses adjacent to the I-205/ MacArthur Drive interchange, take advantage of the freeway-oriented traffic circulation of the site, and serve the local industrial community. The largest parcel (25.5 acres) is located in the northwest corner of the project along Pescadero Avenue. This parcel provides for potential factory outlet expansion or other interstate-related commercial uses. General Commercial land uses provide for service commercial oriented land uses, such as animal shelters, who's land use characteristics are compatible with industrial uses.

### **Right to Farm Provisions**

A number of dairy operations currently exist within the Northeast Industrial planning area. Dairy facilities typically involve significant capital improvements. This Specific Plan intends to allow the continued operation of these existing dairy facilities and other agricultural uses until such time as the owners wish to convert their property to nonagricultural uses.

When nonagricultural land uses move into areas near to pre-existing agricultural operations, the agricultural operations frequently become the subject of nuisance complaints. Chapter 10.24 of the Tracy Municipal Code, the Right to Farm Ordinance, declares that farming operations are not a nuisance, and recognizes the right to farm within the incorporated City. Agricultural and dairy operations are included in the Permitted Uses (page \_\_\_) for both the Light Industrial and General Commercial land use designations.

### **Mineral Extraction**




A number of the parcels in the Northeast Industrial planning area have mineral rights that are owned by different owners than the underlying land. It is possible that these owners may choose to exercise these rights at some time in the future. As such, mineral extraction is included as a Conditionally Permitted use within the Light Industrial land use designation. Any mineral extraction operation will be unobtrusive, and will be a subsidiary use to the primary use of the parcel.

# Northeast Industrial

Figure 6

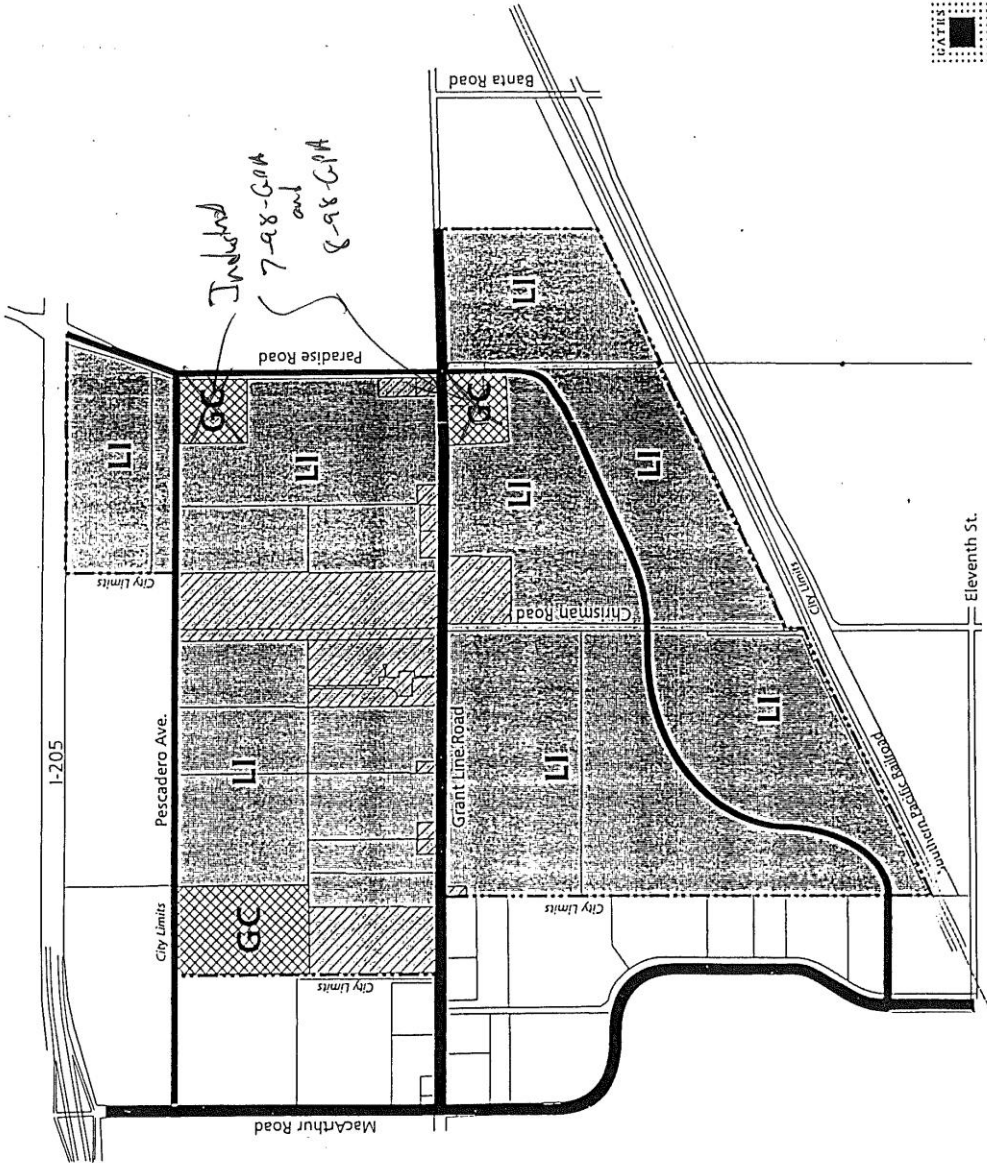
## LAND USE PLAN

### LEGEND

-  General Commercial
-  Light Industrial
-  Existing Dairies and Homes

DEVELOPMENT SUMMARY

| Land Use                | Acres        | FAR  | Square feet       |
|-------------------------|--------------|------|-------------------|
| General Commercial (GC) | 45.5         | 0.35 | 694,000           |
| Light Industrial (LI)   | 798.9        | 0.50 | 17,400,000        |
| Proposed Roads          | 15.8         | -    | -                 |
| Existing Road Expansion | 10.1         | -    | -                 |
| <b>TOTAL</b>            | <b>870.3</b> | -    | <b>18,094,000</b> |



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## CIRCULATION AND TRANSPORTATION

The Northeast Industrial Specific Plan provides for efficient circulation by automobiles and trucks. The proposed land use mix, street geometry, and proximity to the interstate freeway system will minimize project-related impacts to Tracy's transportation network.

The distribution, location and extent of the roadway improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future roadway improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figures X and X show the original roadway network and street sections for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

### **Background Traffic Growth / Traffic Operations**

The City has prepared projections of traffic flows for baseline development. This baseline development includes the Industrial Specific Plan (ISP).

The future background traffic projections have been derived from the Tracy citywide travel demand model. The model's basic assumptions related to trip generation rates and traffic distribution were used in developing the baseline traffic projections. It is noted that model projections have been based on a conservatively high assumption that industrial development has an employee density of 11 employees per acre.

With respect to traffic operations, the ISP improvements and site specific improvements on MacArthur Drive, Pescadero Road, and Grant line Road have been completed. These improvements allow for stable traffic flow conditions (LOS 'C' or better) with buildout of the ISP development.

### **Traffic Increases Due to the Project**

#### **Traffic Generation**

The plan includes primarily industrial development with ancillary (local serving) commercial and a larger commercial retail center which would potentially attract customers from outside the Plan area. The industrial areas are envisioned as primarily warehouse and distribution developments with a smaller amount of more intense light industrial / flex tech development. The following assumptions are provided for the purposes of the Plan's traffic analysis:






- 123 acres of warehouse and distribution development with 8 employees per acre
- 676 acres of light industrial development with an average of 11 employees per acre. (Assumes that 507 acres (75%) are warehouse and distribution uses with 8 employees per acre, and 169 acres (25%) are flex-tech uses at 20-30 employees per acre, resulting in the average of 11.)
- 20 acres of *small* commercial areas designed to serve the internal needs of the Plan area employees
- 25.5 acres of outlet mall type retail development serving subregional customers.

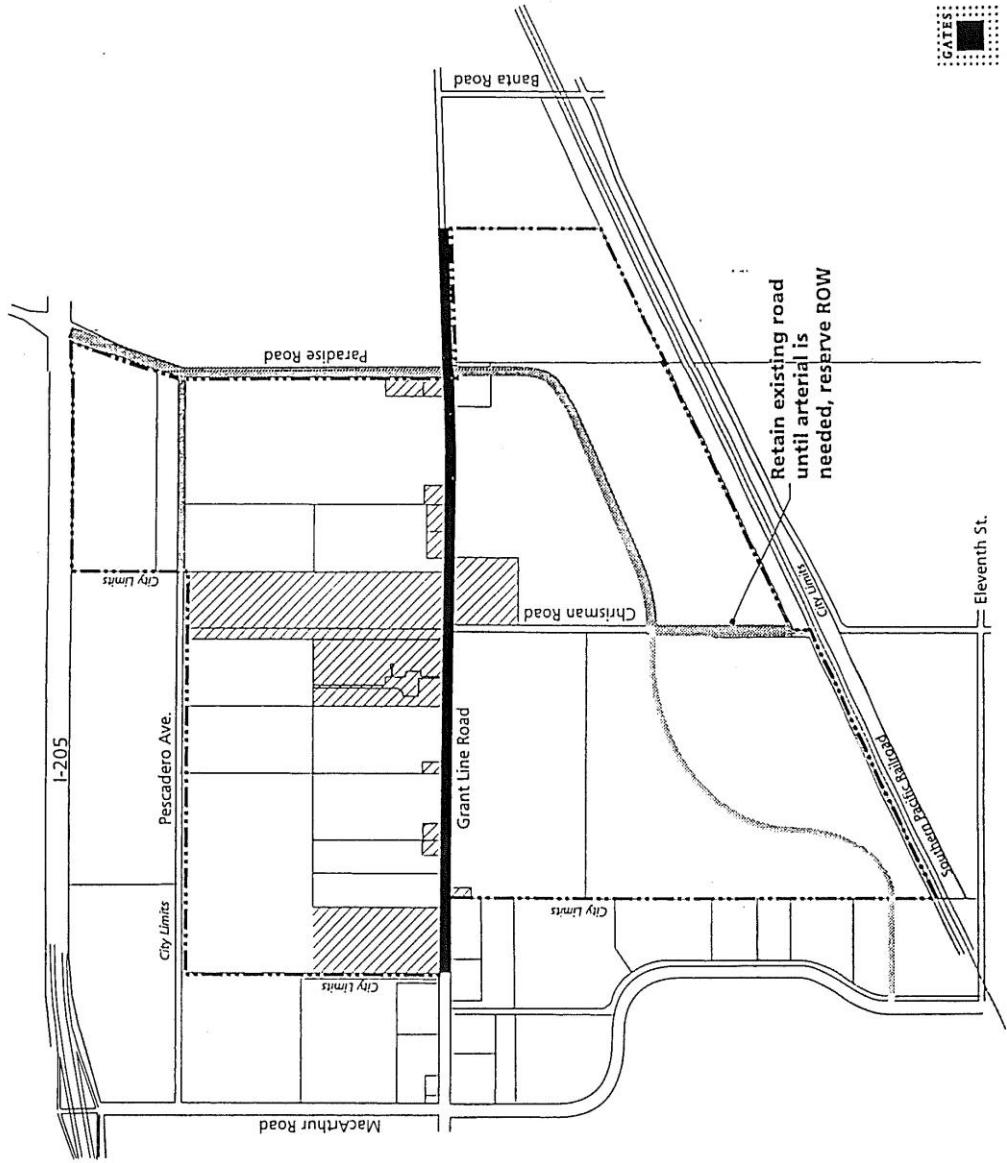


Figure 7

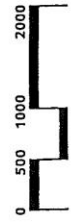
# PROPOSED ROAD NETWORK

## LEGEND

-  4 Lane Arterial, 110' ROW
-  Phase I Arterial Street, 110' ROW
-  Modified Industrial Street, 76' ROW
-  Industrial Street, 60' ROW
-  Existing Dairies and Homes



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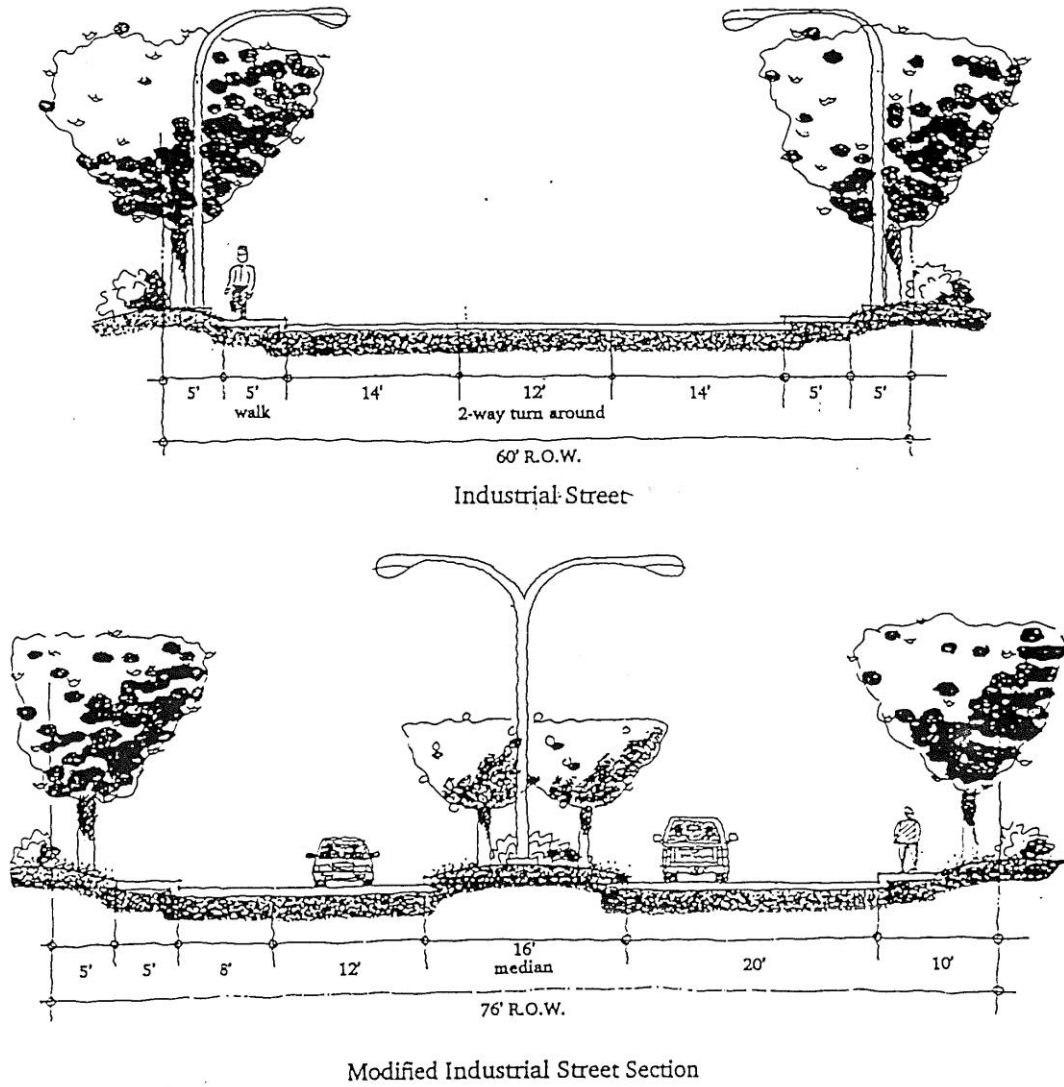


Figure 8A

**STREET CROSS SECTIONS**



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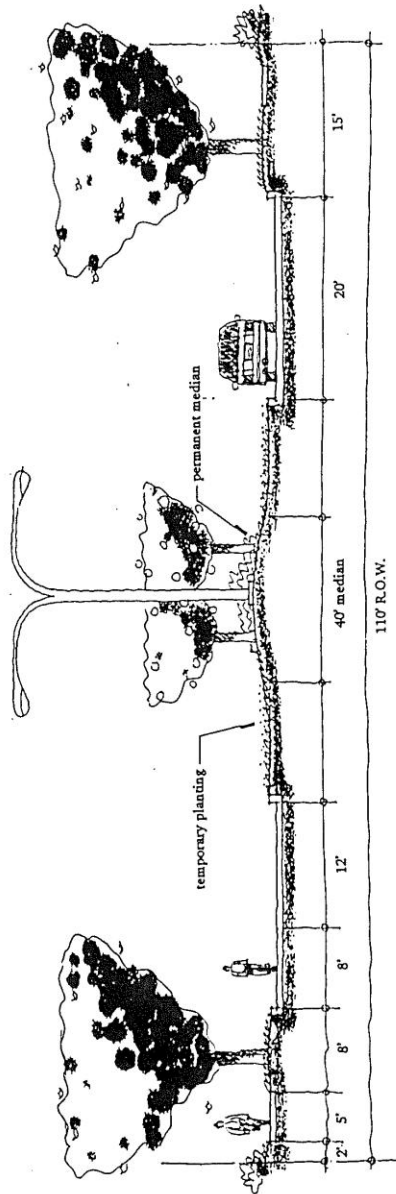




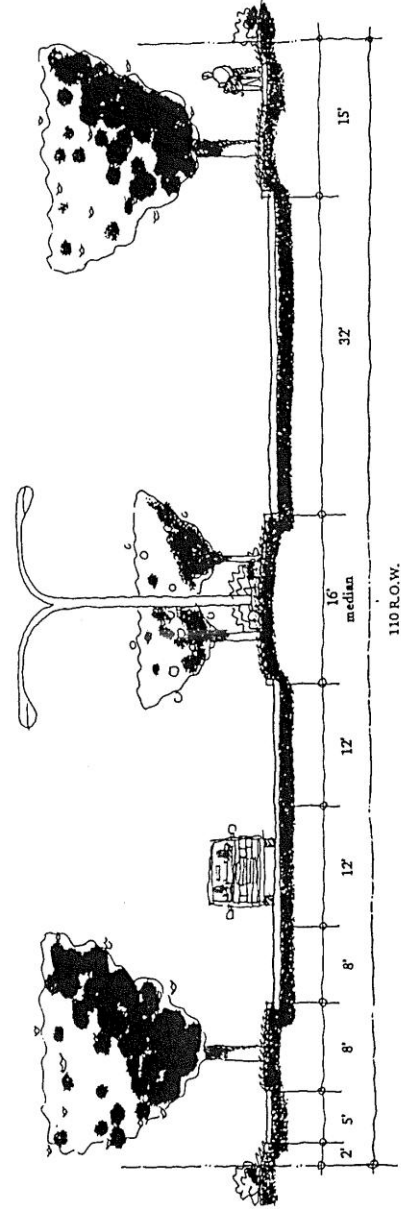


# STREET SECTIONS

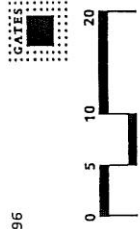
Figure 8B



Phase 1 Industrial Street Section - preserves potential for expansion



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## CIRCULATION PLAN

### Proposed Internal Street Network

As shown in Figure 6, the Plan's internal street network would include existing roadways as well as a new loop street. The street descriptions are as follows:

- Pescadero Avenue would be a two-lane "modified industrial street," with a 76-foot right-of-way, extending from the terminus of the four-lane section easterly through the Plan area to Paradise Road.
- Grant Line Road would be a four-lane "major arterial street," with a 110-foot right-of-way from the terminus of the current four-lane section easterly through the entire Plan area.
- Paradise Road would be a two-lane "Phase I arterial street" extending from the I-205 overcrossing south to Chrisman Road. Beyond Chrisman Road the street would continue to the south and west with a two-lane "Industrial Street" connection to MacArthur Road.
- Chrisman Road would be developed as a two-lane "Industrial Street" north of the loop street to allow convenient parcel access during buildout. South of the loop street, the existing roadway section would be retained in the current condition, with no improvement.
- Additional two-lane "Industrial streets" may ultimately be provided in the Plan area, located in response to specific parcelization and development proposals in the area. Spacing between industrial street intersections would be at least 660 feet.

Figure 7a & 7b indicates the proposed cross sections of the internal streets.

### Parcel Access

Because the Plan does not include specific development parcel details (these will occur as the Plan is implemented), it would be tenuous to identify specific site access. However, the City has studied access issues (Conceptual Design Standards for the Tracy Roadway Master Plan, Fehr & Peers Associates, Sept. 22, 1994). Within the context of these recommended standards, access on the major Plan area roadways (Grant Line Road and Paradise Road-Loop Street) should adhere to the driveway spacing guidelines found in the following Design Guidelines:

### Rail Access

A number of parcels front the Southern Pacific Railroad right-of-way to the south of the planning area. Railroad spurs accessing the rail line may be provided for rail-dependent industries that located in this part of the Planning area.

### Truck Routes

Grant Line Road and MacArthur Drive are currently designated as truck routes in the General Plan. While trucks may use other streets inside the area, such use would only be as needed to access a specific destination. Truck traffic towards destinations outside of the Northeast Industrial area will be directed towards the designated truck routes.

### Relationship Between the Proposed Circulation Plan and the Tracy Roadway Master Plan

The City of Tracy Roadway Master Plan (RMP) was prepared in response to long-term buildout of the City's General Plan. As such, this roadway system was intended to serve 35 years of growth, with an ultimate system (involving right-of-way preservation) that would probably not be needed for 40-50 years.

The two major RMP corridors through the Plan area are Grant Line Road (east-west) and Chrisman Road (north-south). The RMP envisioned that with ultimate long-term buildout, these roadways would carry heavy through volumes due to extensive development in the east areas of Tracy. The Grant Line expressway was intended to carry heavy through volumes to and from I-5, east of the City. The Chrisman expressway was intended to provide an alternative to MacArthur Drive, carrying heavy through volumes to and from I-205 (with a new I-205 interchange).

Because the RMP ultimate system is based upon extremely long-term growth projections, it is not feasible for the proposed Circulation Plan to precisely coincide with the ultimate system. The Circulation Plan differs from the ultimate RMP for two distinct reasons. First, the RMP envisions major six-lane expressways along Chrisman Road and Grant Line Road through the Plan area. Reserving the extremely wide rights-of-way for these roadways would bisect the Plan area (in both directions) and significantly impact the flexibility of future parcelization and development. More importantly, the expressways would create substantial development frontages with extremely limited access (as per the RMP recommendation). This severely restricted access would further impact the ability to market and develop parcels within the Plan area.

Nonetheless, the proposed Circulation Plan does recognize the need to provide the potential for future expansion to wider roadways. As shown in Figure 6, the Circulation Plan would exactly follow the Grant Line Road alignment, providing a four-lane major arterial street through the Plan area. Within the arterial street 110-foot right-of-way, the cross-section could be modified (through a narrow median and elimination of shoulder areas) to accommodate the potential need for six lanes.

A number of viable dairy facilities are in the path of a direct Chrisman Road access to I-205. Due to the disruptive impacts of such an alignment, the Circulation Plan proposes to allow for an alternative north-south roadway in the Chrisman Road – Paradise Road corridor. Although the Circulation Plan only indicates a two-lane modified industrial street, the Plan does provide for a right-of-way width that would accommodate a potential four-lane major arterial. Again, modifications to the four-lane arterial could allow for six lanes if such capacity is ever needed. This alignment also allows for an ultimate I-205 interchange at the existing Paradise Road overcrossing of I-205. It is noted that if an interchange is ultimately located at Paradise Road, it would be about 1.5 miles east of the MacArthur Drive interchange and this spacing would satisfy State and Federal Standards.

Table 8 provides a summary comparison of the Northeast Industrial Circulation Plan, the future expansion potential of the Plan, the RMP 35-year roadways and the RMP ultimate roadways.

*Table 8: Comparison of Circulation Plan*

| <b>Roadway Segment</b>                                      | <b>Northeast Plan</b>            |                              | <b>Roadway Master Plan</b>     |                                |
|---|----------------------------------|------------------------------|--------------------------------|--------------------------------|
|   | <i>Proposed Circulation Plan</i> | <i>Potential Expansion</i>   | <i>35-Year Scenario</i>        | <i>Ultimate</i>                |
| Grant Line, between MacArthur and north-south through route | 4-lane arterial (110-ft ROW)     | 6-lane arterial (110-ft ROW) | 4-lane expressway (116-ft ROW) | 6-lane expressway (140-ft ROW) |
| Grant Line, east of north-south through route               | 4-lane arterial (110-ft ROW)     | 6-lane arterial (110-ft ROW) | 4-lane expressway (116-ft ROW) | 6-lane expressway (140-ft ROW) |

|   |                                       |                              |                                |                                |
|---|---------------------------------------|------------------------------|--------------------------------|--------------------------------|
| North-south through route, south of Grant Line          | 2-lane industrial street (110 ft ROW) | 6-lane arterial (110 ft ROW) | 4-lane expressway (116 ft ROW) | 6-lane expressway (140 ft ROW) |
| North-south through route, between Grant Line and I-205 | 2-lane industrial street (110 ft ROW) | 6-lane arterial (110 ft ROW) | 4-lane expressway (116 ft ROW) | 6-lane expressway (140 ft ROW) |

**Phasing of Roadway Section**

As discussed above, the Northeast Industrial roadway network does provide for right-of-way width that would accommodate a potential north-south major arterial connection to I-205. The Northeast Industrial Specific Plan reserves a 110 foot major arterial right-of-way along the Chrisman Road/Paradise Road alignment for the future north-south connection. As shown in the Phase 1 Arterial section, the 40 foot median would be narrowed to 16 feet, thus creating an additional two 12 foot lanes. The temporary section of the median would be planted with low cost, low maintenance plantings, such as seeded groundcover or grasses.

The section of Chrisman Road south of the loop street will be retained in an unimproved condition until needed as an arterial. A 110 foot wide right-of-way will be reserved to allow for the future expansion.

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**PUBLIC SERVICES AND INFRASTRUCTURE**

**Wastewater Collection and Disposal**

The distribution, location and extent of the wastewater conveyance treatment and discharge within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future wastewater improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure shows the master sewer plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

**City Master Plan**

Figure 9 shows an area which is identified on the City’s Wastewater Master Plan as a part of the tributary area for new sewer pipelines proposed by the Master Plan. Because the City’s Master Plan does not provide any other information, it is assumed the remaining portion of the site is planned to drain into the East Side Trunk.

The City’s Master Plan shows a new 18” diameter sanitary sewer to be installed from Grant Line Road at Chrisman Road, north through the Northeast Industrial planning area and through the Yellow Freight site (the main alignment is only conceptually shown on the Master Plan), then across I-205 to a new pump station and force main which would convey flows westerly to the treatment plan. It appears that most, if not all, the tributary area of the Northeast Industrial area is intended to be served by this new system. If this main is to be built, it would be constructed east of Yellow Freight.

The Master Plan also shows a future 24" and 30" sewer system flowing counter-clockwise generally around the southern, eastern, and northern boundaries of the site and then into the pump station described above. It appears this system is primarily planned to serve other areas, however the City's Master Plan does not provide any detail about shed boundaries, and this system may also be intended to serve some portion of the Northeast Industrial Site.

#### **Sanitary Sewer Serviceability Analysis**

Figure 10 shows a proposed sanitary sewer pipe system layout to serve each parcel within the Northeast Industrial Site. This proposed collection system differs from the City's Master Plan and would discharge the entire Northeast Industrial planning area into the East Side Trunk. If found to be a viable alternative, this may eliminate the need for Northeast Industrial area to construct significant portions of the new systems shown on the Master Plan, particularly the new pump station and 18" force main on the north side of I-205.




Preliminary analysis of the pipe grades and USGS topography maps indicate it may be possible to serve the entire Northeast Industrial planning area with a gravity system which outfalls to the East Side Trunk. The proposed layout shown in Figure 10 would make use of the existing pipes in Grant Line Road and Pescadero Roads to the extent possible. However, due to vertical considerations, it may be necessary to bypass some or all of the existing pipes leading to the East Side Trunk (particularly the 8" pipe in Pescadero Avenue). It should also be noted the area which the existing pipe in Grant Line Road can serve is probably limited, due to grade restrictions caused in part by conflicts with other utilities.

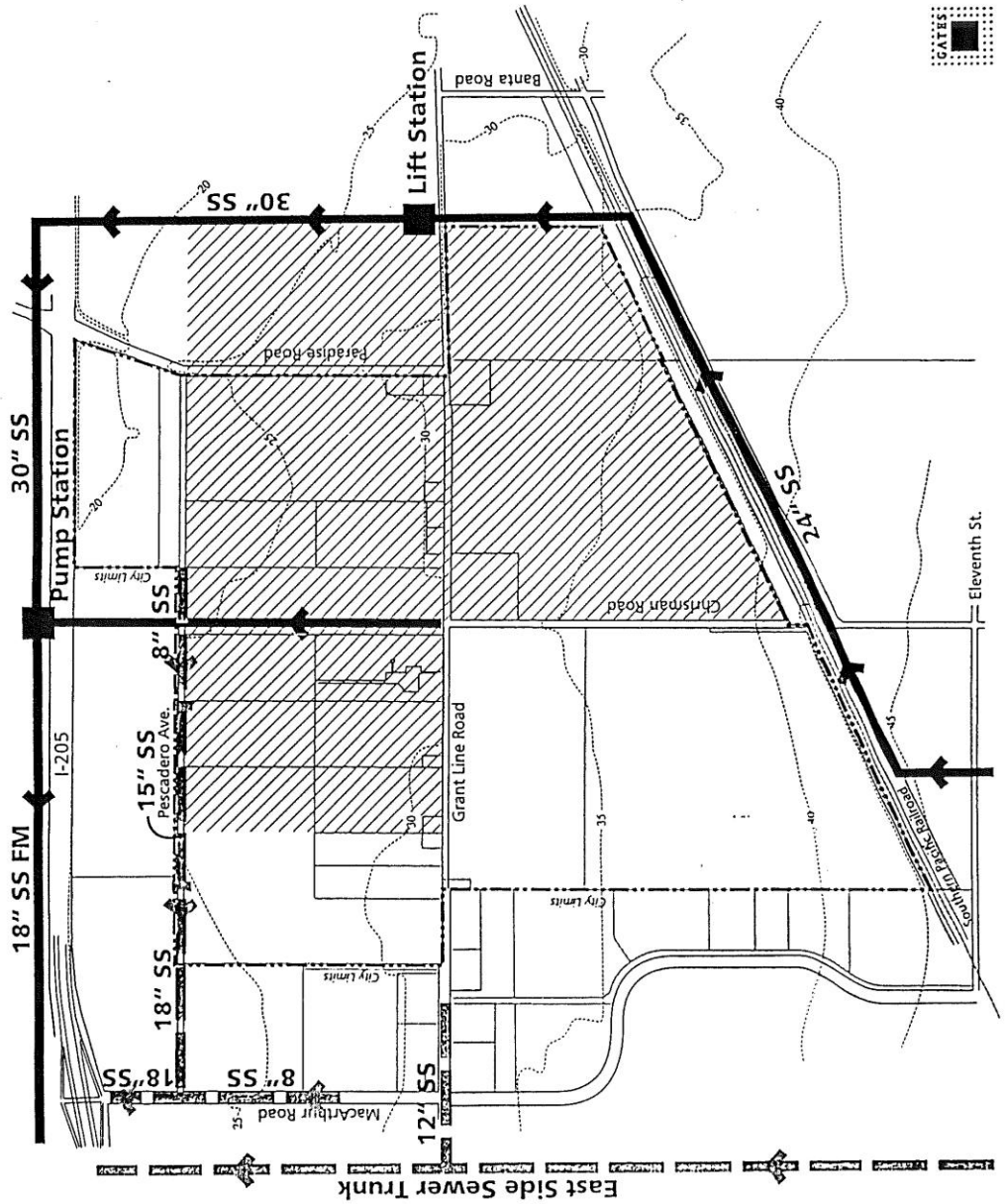


Figure 10

# MASTER PLAN SEWER

## LEGEND

-  Tributary Area for M.P. Sewer
-  Existing Sanitary Sewer
-  Master Plan Sanitary Sewer



Source: City of Tracy Wastewater Master Plan

February 26, 1996






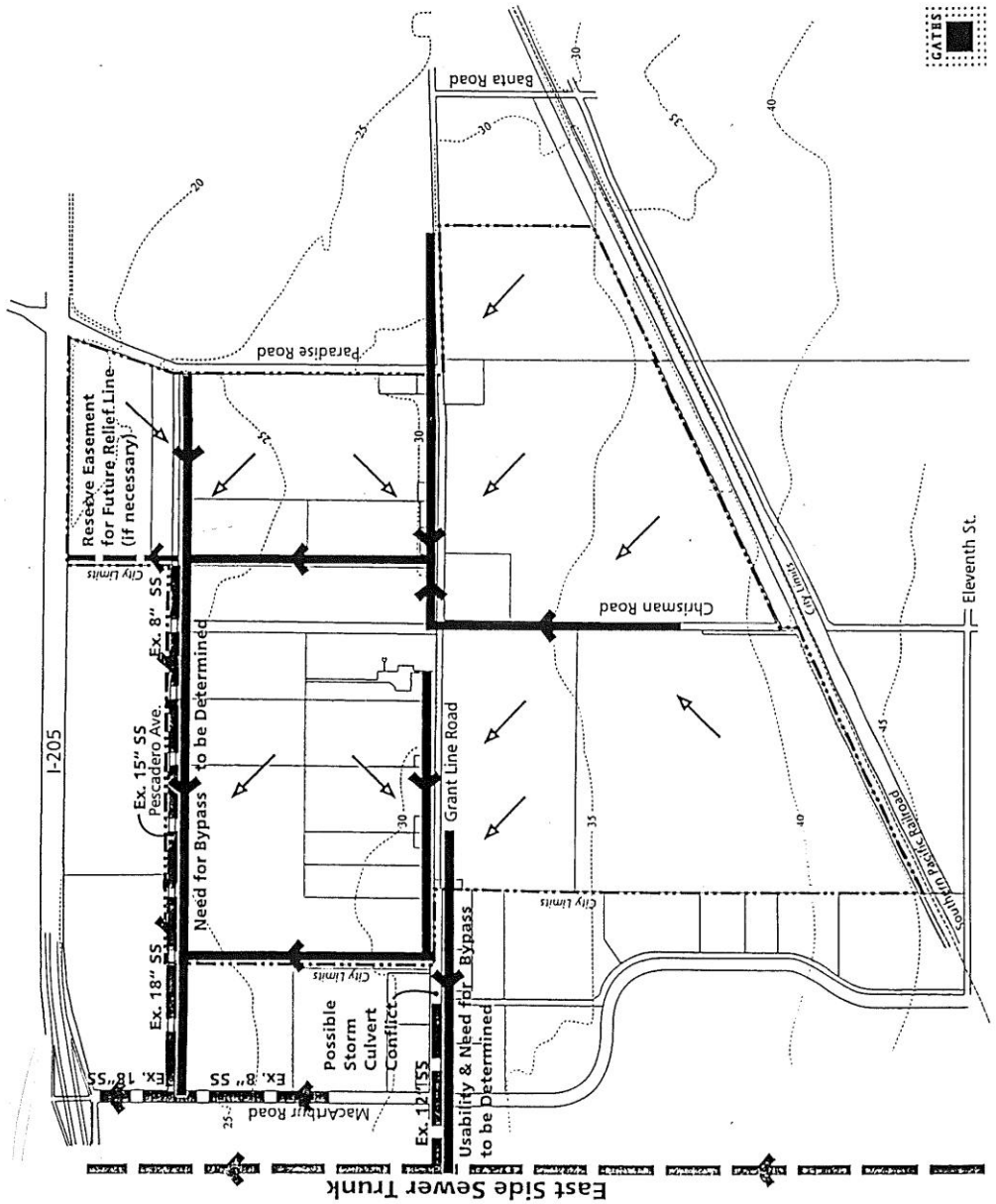


Figure 11

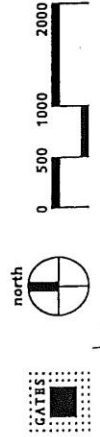
# PROPOSED SEWER SYSTEM

## LEGEND

-  Existing Sanitary Sewer Main
-  Proposed Sanitary Sewer Main
-  Direction to Main Connection for Development Areas



February 26, 1996



Depending on the calculation methodology used for sewage generation, inflow, infiltration and peaking factors, and the assumptions used regarding specific land use and employee density, the Northeast Industrial planning area is expected to have a sewage generation of between 1.5 and 3.0 million gallons per day (mgd). The 3.0 mgd could be exceeded if a high water user is located within the site.

A draft report by CH2M Hill shows the excess capacity of the East Side trunk to be about 3.6 mgd downstream of Grant Line Road. Preliminary calculations indicate the existing 18" sewer main in Pescadero Avenue has a total capacity of about 2.4 mgd and its excess capacity is estimated at 1.8 mgd (current flows in this system were not available from the City Public Works Department and should be verified). The portions of the existing excess capacities in the East Side Trunk and tributary systems, which can be used by the Northeast Industrial planning area, needs to be determined in order to assess the viability of the layout shown in Figure 10.

If it is determined that in the ultimate buildout condition there will not be enough excess capacity in the East Side Trunk to serve the entirety of the Northeast Industrial planning area, it may be possible to temporarily serve a significant portion of the Northeast Industrial buildout via the East Side Trunk. This would allow construction of expensive offsite improvements, such as the new pump station and force main, to be deferred until a later date. A potential capacity shortage could occur as a result of capacity being allocated to others for the siting of a high water use facility within the plan area. It is suggested that easements for additional mains flowing northerly be reserved to allow for future flexibility in providing alternative sewer main scenarios.

If necessary, other alternatives to the City's Master Plan which may be worthy of consideration are to make use of the Heinz pipeline which may be abandoned in the near future and/or to parallel the East Side Trunk.

## **Storm Drainage**

The distribution, location and extent of the storm drainage improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future storm drainage improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure shows the original storm drainage master plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

### **City Master Plan**

As shown on Figure 11, the City's Master Plan identifies a main drainage divide through the middle of the site, approximately along the Chrisman Road alignment. Lands to the west of this divide are planned to flow to the west towards the East Side Channel. Lands to the east of this divide are planned to drain to the east into a future channel which would flow to the north and then north-west paralleling Tom Paine Slough to Sugar Cut.

### **Storm Drain Serviceability Analysis**

The City's current Storm Drain Master Plan for areas east of Chrisman Road will require expensive offsite improvements, and as such, alternative Master Plan drainage solutions for the Northeast Industrial planning area have to be considered. Alternatives which merit consideration include the following:

- a) Drain some or all of the portion of the site which is east of Chrisman Road to the East Side Channel. Due to capacity issues in the East Side Channel, any such shed "diversion" will result in the need for a



detention basin(s) on site. The location and depth of detention basin(s) and channel hydraulics will dictate whether or not pumps will be necessary to drain the pond(s). Groundwater consideration will also influence the size of the ponds.

- b) Drain the eastern portion of the site directly north of the Tom Paine Slough in a newly constructed drainage facility. This alternative may also require one or ore detention basins and/or water quality basins. This will require a revision to the Master Plan and approvals from those responsible for the operation of Tom Paine Slough.
- c) Temporary discharge the eastern portion of the site into existing irrigation/tail water ditches and await the construction of ultimate drainage facilities. This may require detention if temporary facilities are limited in capacity.
- d) Temporarily store water in onsite retention basins and await the construction of ultimate drainage facilities. This concept may include temporary discharge to the East Side Channel or the tailwater ditches. Temporary facilities will need to follow a phasing plan.

As properties within the project area develop, tailwater drainage from the West Side Irrigation District will need to be contained, redirected or otherwise accommodated. The details of how this accommodation might best occur need further study.

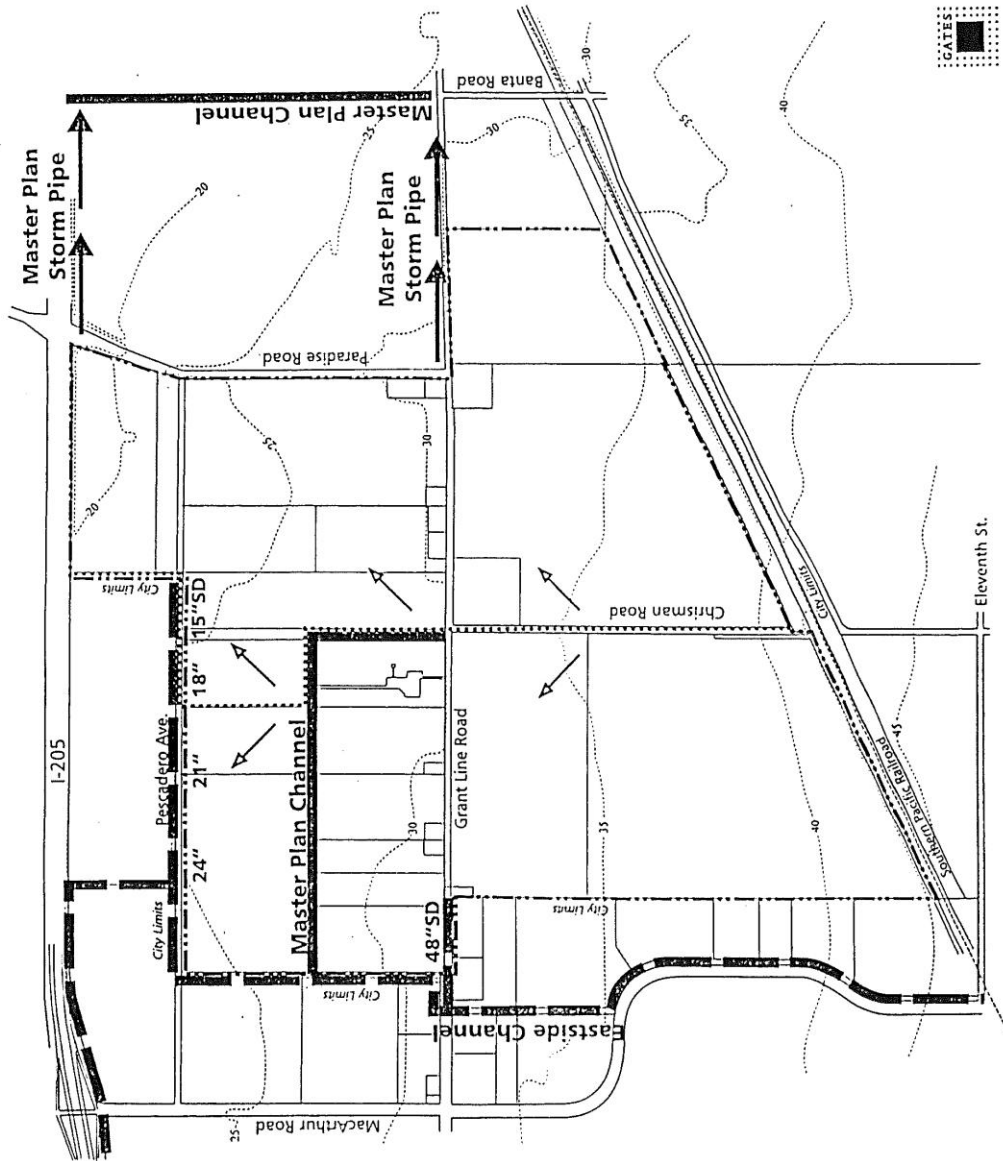


# MASTER PLAN STORM DRAIN

Figure 12

## LEGEND

- Drainage Boundary
- Direction of Drainage
- Existing Storm Drain
- Master Plan Storm Pipe
- Master Plan Storm Channel



Source: City of Tracy Storm Drainage Master Plan

February 26, 1996



## Water Supply and Distribution

The distribution, location and extent of the water improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future water improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure shows the original water master plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

### Water Supply

Currently, the City uses a blend of treated surface water and untreated groundwater. The City of Tracy Water Master Plan currently proposed using surface water sources exclusively to meet water supply demands. The Master Plan indicates that the existing groundwater supply will be maintained only for emergency usage. The City plans to increase their water treatment capacity to both meet the increased demand associated with the projected population growth and to allow additional surface water supplies to be used in lieu of the current groundwater supply.

### City Master Plan Distribution Network

The City Water Master Plan, a portion of which is shown on Figure 12, shows a network of mains varying in diameter from 12" to 18" throughout the site. A 12" to 16" diameter main is planned along Grant Line Road, to the east end of the site, and beyond. An 18" diameter water main is planned to run along Chrisman Road. A 12" diameter main is planned along Paradise Road. The Master Plan also calls for a 12" diameter water main loop in the southeast portion of the Plan area.

### Water System Serviceability Analysis

Figure 13 shows a likely refinement to alignments shown on the City's Water Master Plan which takes into consideration the proposed Northeast Industrial roadway network. The size and location of water lines will need to be reviewed as development plans for the site and surrounding areas progress.

### Police Protection

Police services to the project will be provided by the City of Tracy Police Department.

### Fire Protection

The planning area is located within the boundaries of the Tracy Fire Department. The Tracy Fire Department will provide fire protection service and paramedic ambulance service to the planning area.

### Other Utilities

Pacific Gas and Electric (PG&E) will provide electricity and natural gas to the planning area. Pacific Bell will provide telephone service to the planning area from the Central Office located in Tracy.

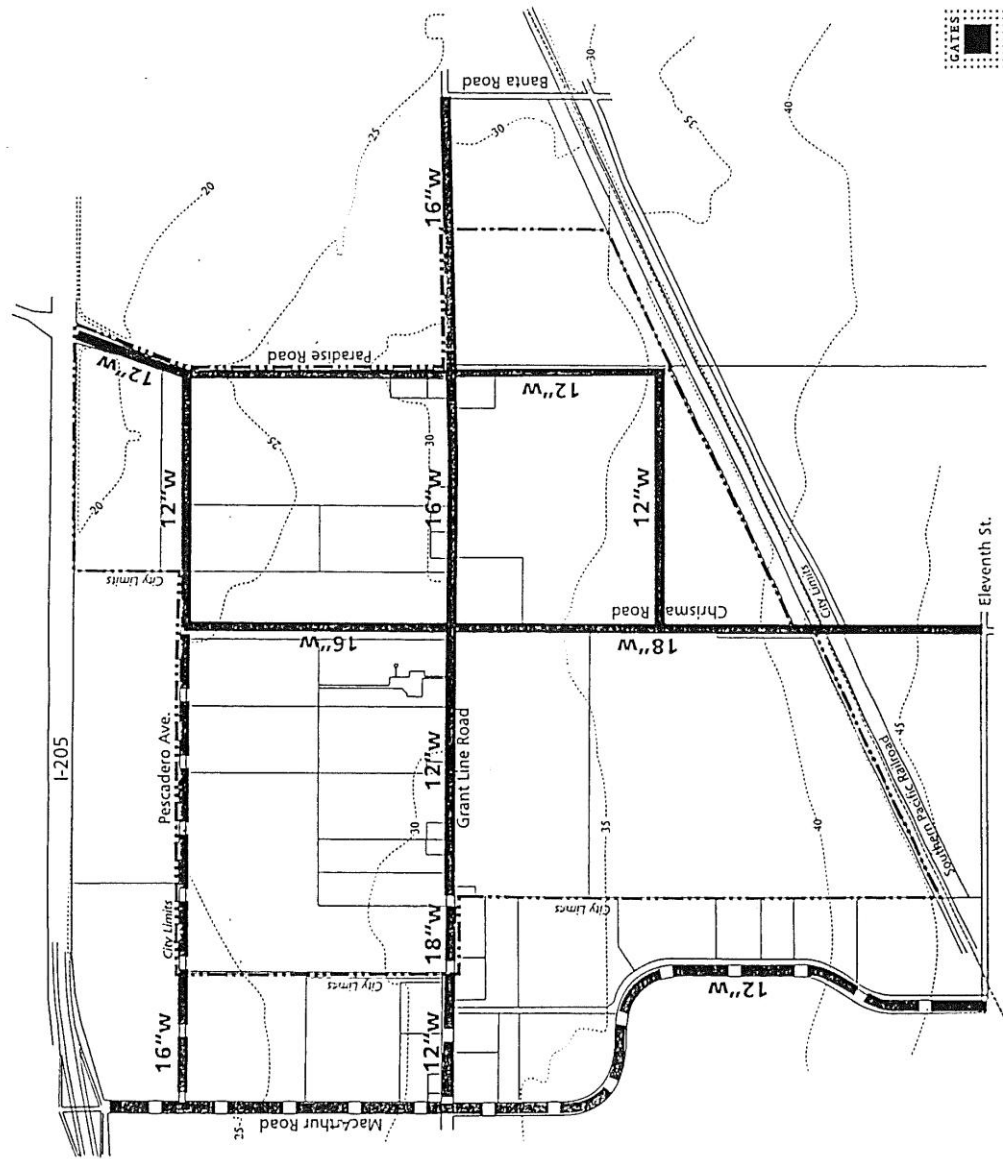


# MASTER PLAN WATER

Figure 13

## LEGEND

- Existing Water Main
- Master Plan Water Main



Source: City of Tracy Water Master Plan

February 26, 1996



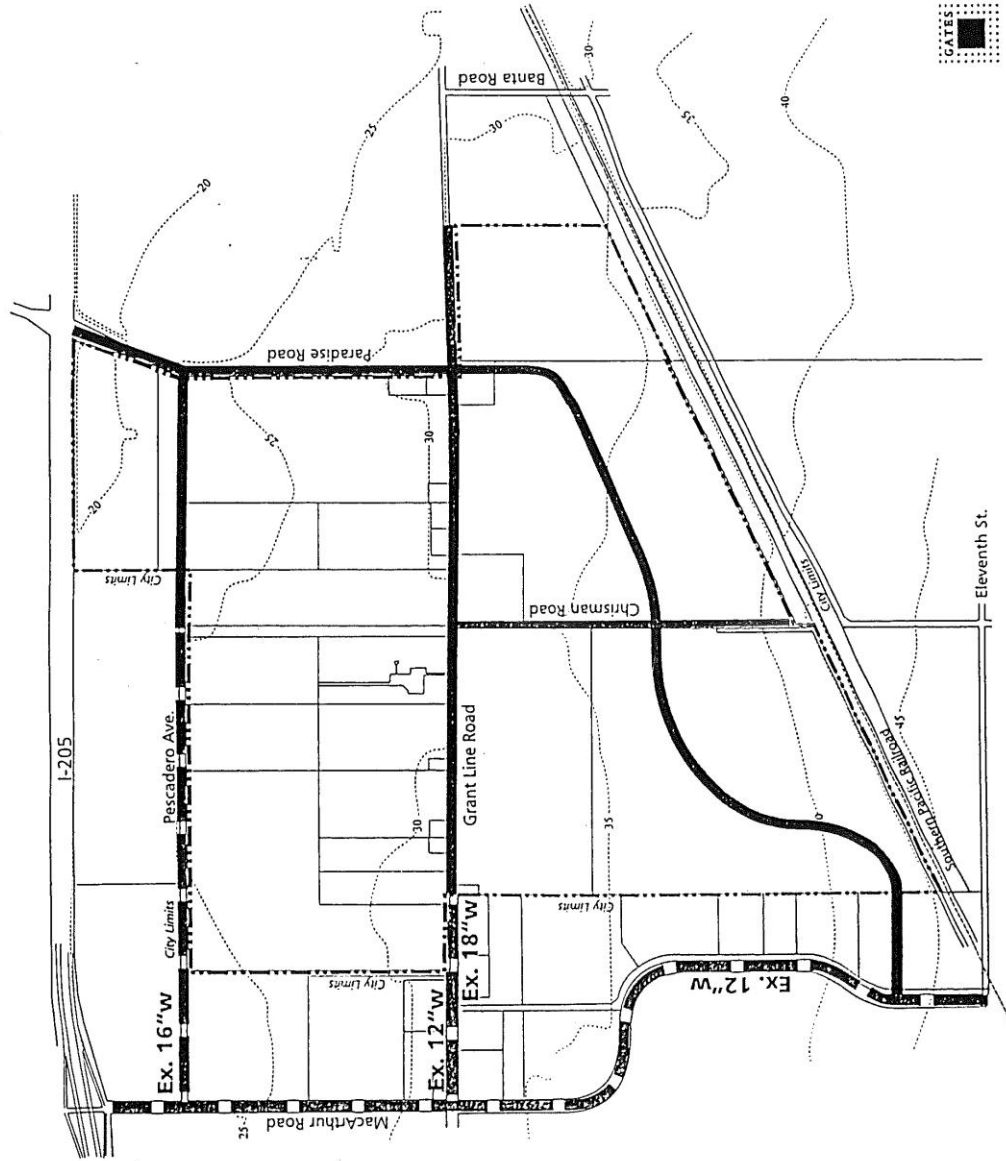


Figure 14

# PROPOSED WATER SYSTEM

## LEGEND

- Existing water Line
- Proposed Water Line



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## PHASING AND FINANCING

### Phasing

The presence of the existing dairies and residences leads to a natural division of the Northeast Industrial planning area into several phases.

### Financing and Fiscal

The proponents of the Northeast Industrial Plan, like the I-205 Corridor Specific Plan and a number of other projects before it, will work the City staff to arrive at a plan to finance the infrastructure needed to serve the project. Toward that end, a draft Financing Plan will be submitted within a reasonable time of submitting this document. The Financing Plan will identify what facilities are likely to be needed, what their estimated costs are, and how they might be paid for. It is anticipated that this plan will include a combination of impact fees that community facility districts to pay for the improvements over time. It is not considered likely that the project will cause a fiscal impact to the City that cannot be funded within the provisions of the property tax rate.

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## DESIGN GUIDELINES

The Specific Plan includes design guidelines and development standards to guide site planning and architecture. These design guidelines are essentially the same as similar to those adopted in the Industrial Areas and I-205 Corridor Specific Plans, which are adjacent to the planning area.

### Streetscapes

1. The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, berming, lighting, and signage to produce an attractive and functional environment.
2. All landscaping should employ a mix of trees, shrubs, groundcovers and turf, where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plans of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs, and turf is encouraged, and compliance with the state's water efficient landscape guidelines is required.
3. The use of lawn substitutes is encouraged in all medians and for parkways. The use of turf should be minimized and reserved for areas of high use or visibility and temporary median planting in anticipation of future street widths.
4. Automatic irrigation is required for all landscape areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, and parking areas, etc. The use of water conserving systems, such as drip irrigation for shrub and tree planting, is encouraged.
5. Tree plantings should reflect street hierarchy with larger trees along arterial streets and smaller trees on industrial streets. Tree plantings shall be symmetrical and of the same species in the parkways on both sides of the streets. One tree species or mixture of species shall be planted consistently at regular intervals along the entire length of a street. Spacing interval shall be no greater than 40 feet on center. Where trees are planted in medians, the plantings shall be continuous and at regular intervals. Spacing of median trees shall be no greater than 30 feet on center. Different tree species shall be planted at intersections to highlight these areas.
6. Adequate sight lines shall be maintained at all times

### Recommended Trees for Major Streets

The following list identifies recommended trees for the major streets that form the framework of the area:

| <b>Street</b>        | <b>Parkway</b>                | <b>Median</b>                 |
|----------------------|-------------------------------|-------------------------------|
| Grant Line Rd        | Pistacia chinensis            | Prunus serrulata cvs.         |
|                      | Chinese Pistache              | Flowering Cherry              |
| Paradise Rd/ Loop Rd | Celtis sinensis               | Pyrus calleryana 'Bradford'   |
|                      | Chinese Hackberry             | Bradford Pear                 |
| Pescadero Ave        | Fraxinus oxycarpa 'Raywoodii' | Pyrus calleryana 'Aristocrat' |
|                      | Raywood Ash                   | Aristocrat Pear               |

Select one of the following street trees for use on each industrial road:

| <b>Botanical Name</b>         | <b>Common Name</b> |
|-------------------------------|--------------------|
| Eucalptus gunnii              | Cider Gum          |
| Fraxinus oxycarpa 'Raywoodii' | Raywood Ash        |
| Fraxinus uhdei                | Evergreen Ash      |
| Platanus acerifolia           | Sycamore           |

**Street Lighting**

1. Illumination standards for arterial and industrial streets should reflect the different right-of-way widths and functions.
2. Light fixtures and standards shall meet all safety standards and shall be employed throughout the length of the street. It is recommended that one lighting fixture style be employed for use on all streets. ~~The preferred style is the Davit pole painted grey green.~~
3. Where possible, light standards shall be located in medians.

**Building Floor Area Ratio and Height**

| <b>Land Use</b>    | <b>Max. Floor Area Ratio</b> | <b>Max. Building Height</b> |
|--------------------|------------------------------|-----------------------------|
| General Commercial | 0.35                         | 46 ft.                      |
| Office             | 0.35                         | 46-60 ft.                   |
| Light Industrial   | 0.50                         | 46-60 ft.                   |

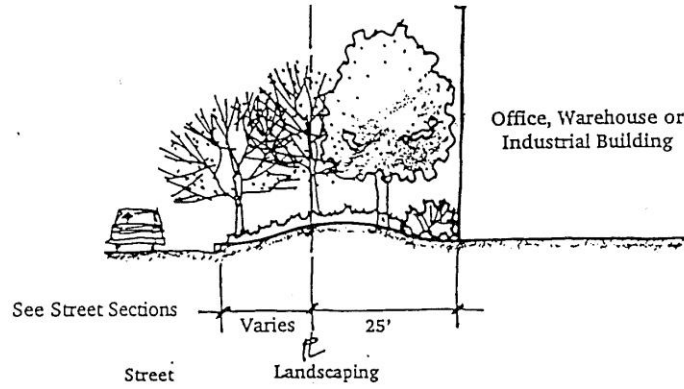
**Building Setbacks**

The following shall be the minimum building and parking setbacks required for all building types. Figure 14 illustrates these guidelines.

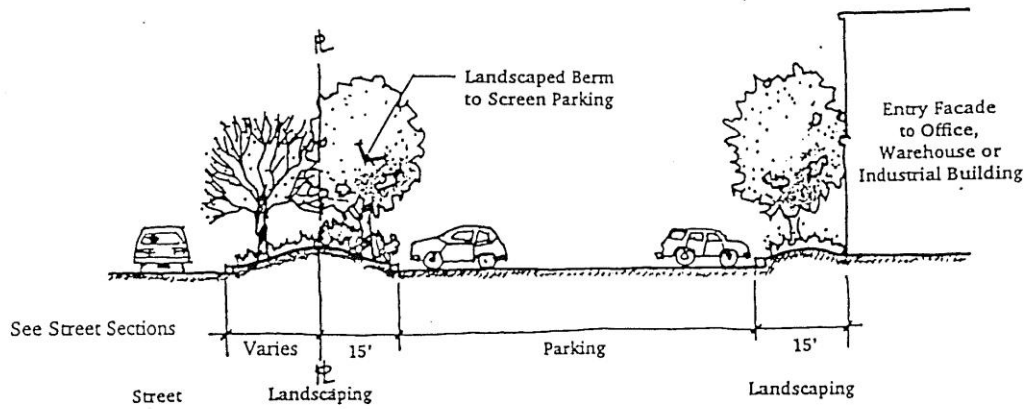
1. Building setback from any property line adjacent to a street or Caltrans right-of-way shall be 25 feet minimum. Rear and side yard building setbacks from property lines not adjacent to a street or Caltrans right-of-way shall be 15 feet minimum.
2. A 5 foot wide landscape setback is required along property lines not adjacent to a right-of-way. On the property lines perpendicular to the street frontage on industrial streets, the landscaped setback is only required to a point 150 feet onto the parcel from the street right-of-way or 50 feet back of building face, whichever is greater.
3. Parking setback from any property line along a public street or the Caltrans right-of-way for commercial land uses shall be 10 feet and for industrial uses shall be 15 feet.

4. Parking shall not be permitted within 10 feet of the building entry face of any commercial structure. In the event the building has an arcade or other shade structure along this frontage, the structure can be located within this required setback. Parking shall not be permitted within 15 feet of the office face or portion of a building. On industrial buildings, a 15 foot setback to the parking area shall be provided at building entries.
5. Commercial buildings shall be sited so as to create and enhance the streetscape. This can only be accomplished if all or a portion of the buildings are located near the street. On commercial sites of over 4 acres, at least one building must be located with a minimum setback from public-right-of-way to building face of 50 feet. On corner sites adherence to this requirement is encouraged on both frontages, however, only required on the major street frontage.

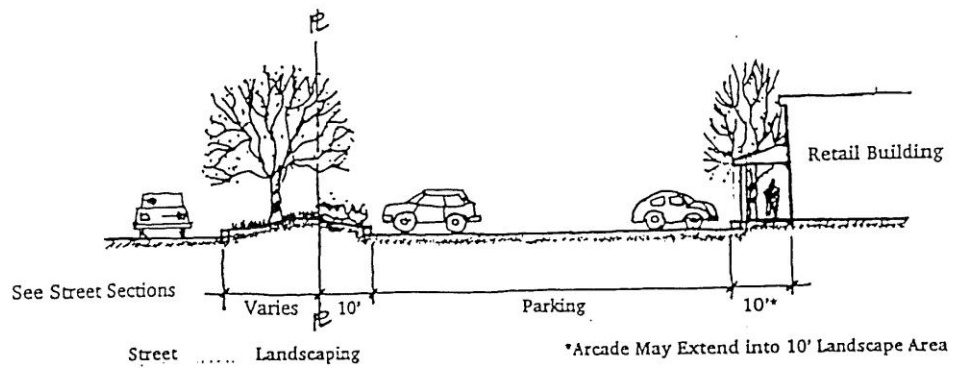




Front Yard Setback: Without Parking



Front Yard Setback: With Parking



Front Yard Setback: With Parking

Figure 15

### SETBACK GUIDELINES



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**Parking and On-Site Vehicular Circulation**

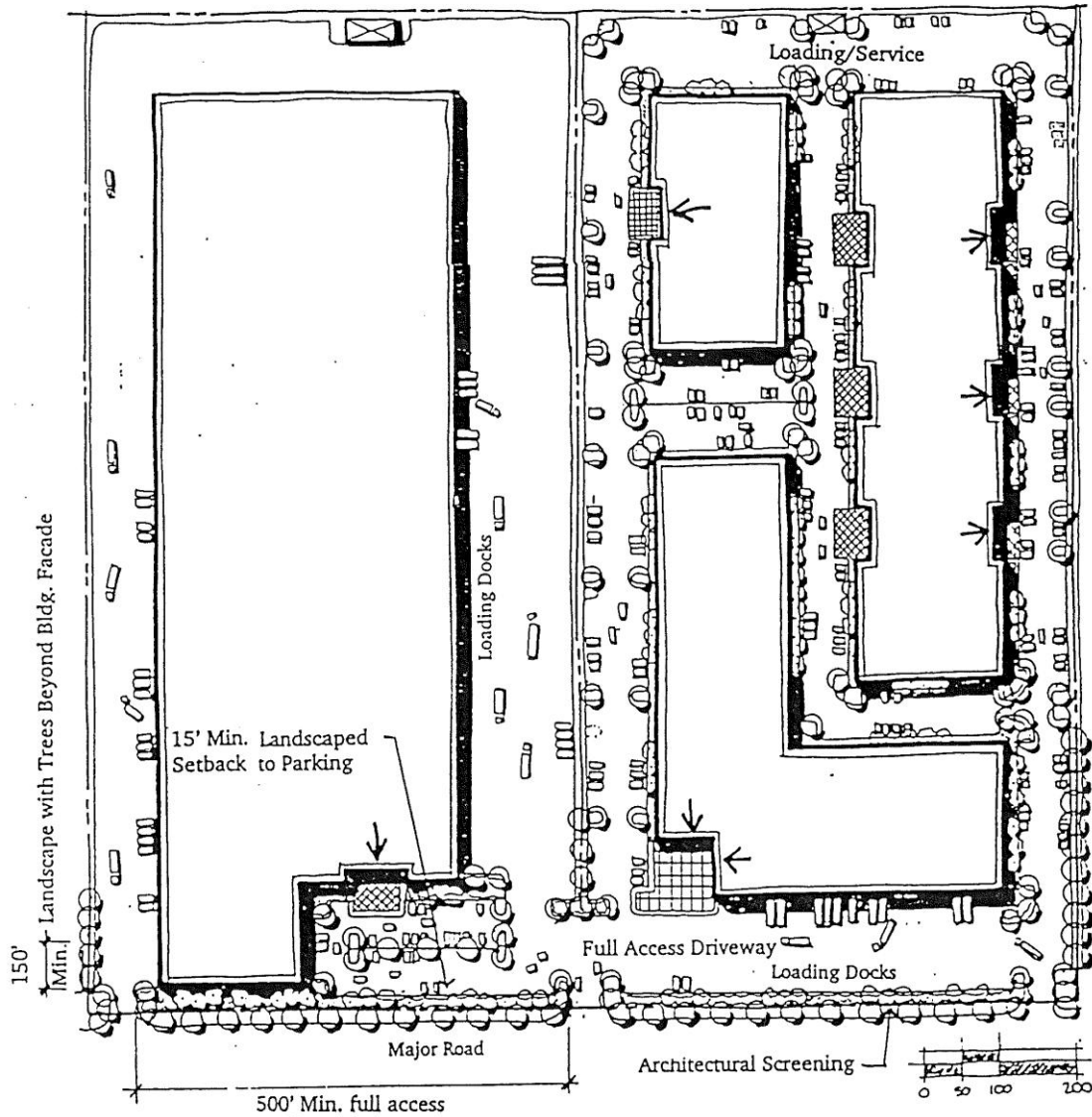
1. Parking, on-site circulation, and loading area standards shall be as required by the provisions of Title 10, Article 26, Off-Street Parking Requirements of the Tracy Municipal Code unless modified below or as part of the Final Development Review Plan approval. Portions of off-street parking requirements are summarize below.
2. Parking lots containing 10-20 spaces may include a maximum of 20% of the total number of spaces for compact cars. These spaces shall be designed and marked in accordance with City standards and distributed throughout the lot. Compact car space parking areas containing 20 or more spaces may include a maximum of 30% of the total number of spaces for compact cars.
3. Minimum off-street parking standards:

| <b><i>Uses</i></b>  | <b><i>Minimum Parking Spaces Required</i></b>  |
|---|--|
| Retail  | One space per 250 square feet of gross floor area  |
| Vehicle sales and rentals, including recreational vehicles and mobile homes                     | One space per 250 square feet of gross floor area plus one space per vehicle for sale or stored on lot.  |
| Office: business, professional (not including medical or dental), banks                         | One space per 250 square feet of gross floor area  |
| Dental and medical clinics or office  | One space per 200 square feet of gross floor area  |
| Motor vehicle repair garages  | One space per 600 square feet of gross floor area; repair stalls not counted as parking spaces   |
| Cafes, restaurants, and other establishments for the sale and consumption of food and beverages | Dining: one space per 45 square feet of customer area and one space per 250 square feet of all other area, plus additional spaces connected to uses such as drinking establishments. Drinking bars, cocktail lounge: one space per 35 square feet of drinking, bar, lounge area. |
| Manufacturing   | One space per 600 square feet of gross floor area, or if the number of employees on the maximum work shift can be verified, one space per one employee on the maximum work shift   |
| Warehouses/Storage and Distribution   | One space per 1,000 square feet of the first 20,000 square feet of gross floor area, plus one space per 2,000 square feet of the second 20,000 square feet of gross floor area, plus one space per 4,000 square feet of the remaining square feet of gross floor area.           |

**Loading and Unloading Spaces**

1. Sufficient off-street loading and unloading spaces shall be provided on each site, and adequate provisions and space shall be made for maneuvering freight vehicles and handling all freight. All loading activity, including turnaround and maneuvering, shall be made on site.

2. In commercial areas, truck loading areas and docks shall not be permitted between building(s) and the public street unless enclosed with architectural screen of material similar to building.
3. In industrial areas, truck loading areas and docks shall not be permitted between building(s) and the street unless the building(s) are set back from the curb a minimum of 125 feet and doors are screened by landscaping, berms, and/or fences.
4. Buildings, structures, and loading facilities shall be designed and placed upon the site so that vehicles, whether rear loading or side loading, may be loaded or unloaded at any loading dock, door, or area without extending beyond the property line.



Warehouse / Storage / Distribution  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 213,000 s.f.  
 73 Parking Stalls

Light Manufacturing / R&D  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 193,000 s.f.  
 429 Parking Stalls

Figure 16

**LIGHT MANUFACTURING / R&D PROTOTYPE**



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## Driveway Standards

Driveways should be carefully located so as not to impede the primary function of the streets, which is to carry through traffic. It should be noted that these spacing guidelines are minimum values. The goal should be to exceed them where possible.

1. Individual industrial parcels on major arterial streets may have driveways, but they should be carefully located so as not to impede the traffic efficiency. In general, parcels with frontage on the major arterials should have their entryway on side streets if possible. If a parcel's only frontage is on the major arterial, every effort should be made to consolidate access at a single driveway. Spacing standards for driveways on major arterials shall be as follows:
  - a. Full access driveways, 500 ft. minimum
  - b. Partial access driveways (right in/out, left turn in), 500 ft. minimum
  - c. Right turn in and out, 350 ft. minimum upstream from an intersection
  - d. Right turn in and out, 200 ft. minimum downstream from an intersection
2. On industrial streets, spacing for full access driveways is 450 feet, minimum. "T" intersections are encouraged over four-way intersections. Every effort should be made to consolidate driveways.
3. No driveway shall be located closer than 200 feet to the radius return point at intersections.
4. Driveways shall be a minimum of 25 feet wide. Subsequent development shall demonstrate driveway width and placement can accommodate truck turning movement and clearing without blocking roadways.
5. Driveway width modifications may be approved with shared (ganged) driveways. Ganged driveways which serve two adjacent sites will be required to install landscaped islands along parking adjacent to the gang driveway and a landscape zone at the end of the common drive will act as a terminus to the view line down the ganged driveway.
  - a. Full curb returns (as opposed to a standard driveway) shall be utilized for entries to all sites of over ten acres in size or for common driveways that serve two adjacent sites that together total more than ten acres.
6. Access driveways shall provide adequate length to accommodate off-street vehicle stacking needs during times of peak use.
7. Parcel entry should be clear, attractive, and inviting; circulation should direct [employee](#) and visitor traffic clearly through the site to main building entries and drop-off points and service trucks to loading.
8. In commercial areas, vehicular entries to the site shall be well defined and recognizable to motorists. Improvements should include accent paving, signs, special plantings, and lighting. Such improvements shall not block motorists' sight lines to oncoming traffic.

## Freeway Interface

The control of views of Tracy from I-205 is critical for the establishment of a quality image for the community.

1. Locate services and storage areas to minimize visibility from I-205.
2. All freeway setback zones shall be planted with a combination of trees, shrubs, and groundcover. Automatic irrigation is required of all planted area. Use large scale trees, from a 24-inch box minimum, grouped in single species clusters. Mass trees to avoid blocking views of commercial signage while providing at least one tree per 1,500 square feet of setback area. Plant shrubs in an informal hedge near the property line with gaps between hedges of 50 feet maximum. Install from 5 gallon cans, minimum, in single species clusters at least 100 feet long. Hydroseed or otherwise install permanent groundcover in all places not planted with shrubs.

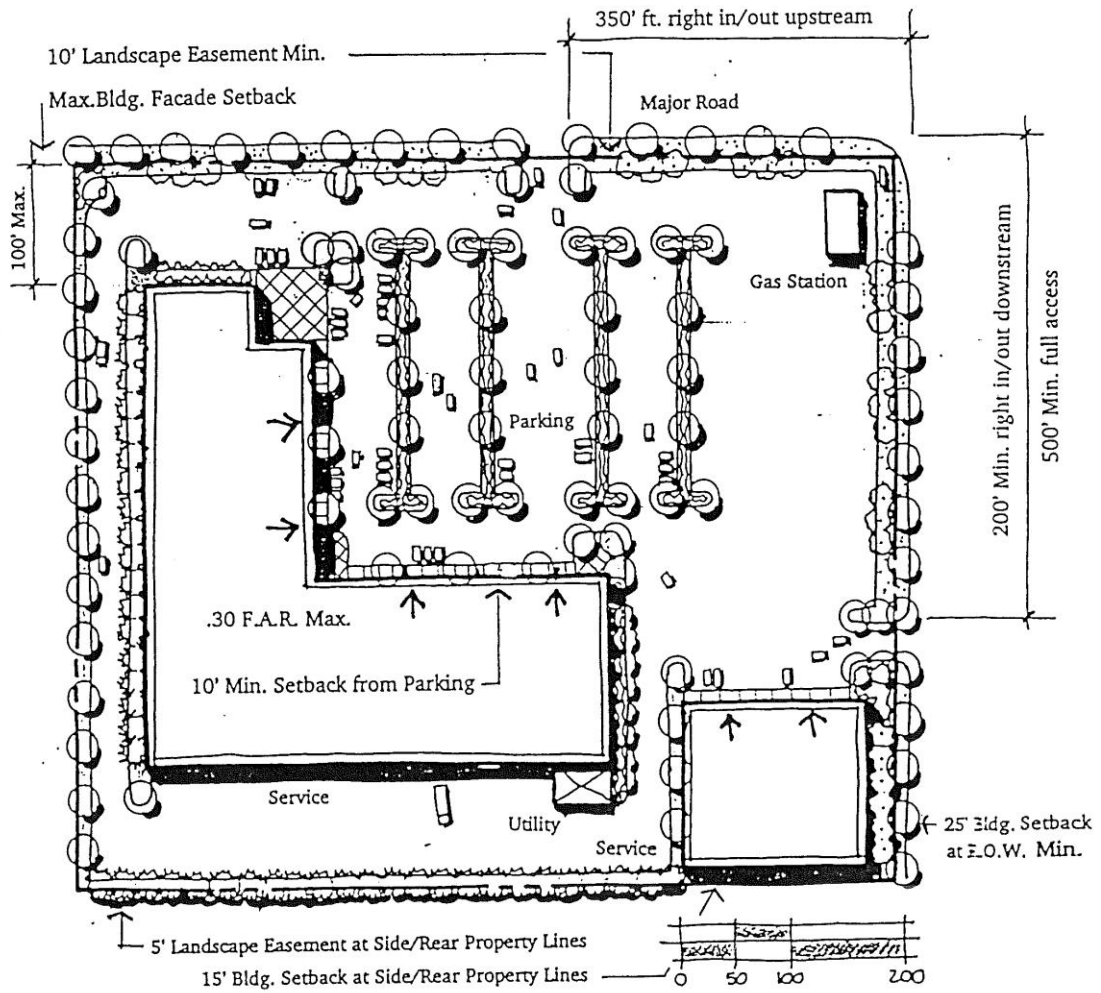


Figure 17

### COMMERCIAL PROTOTYPE



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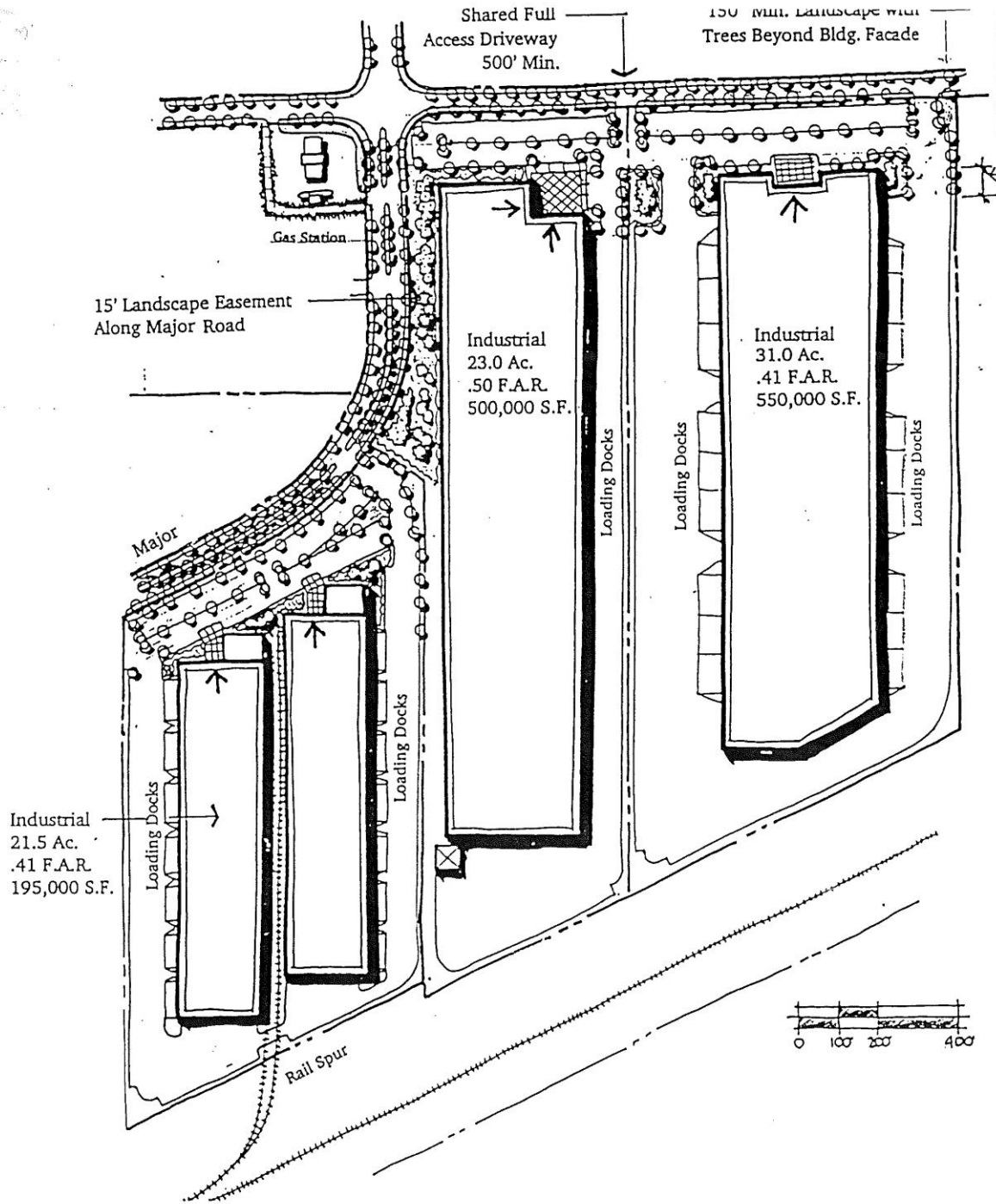


Figure 18

### WAREHOUSE PROTOTYPE



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## Building Architecture

1. Use of creative building design and construction techniques is encouraged. Special attention should be given to that portion of the building visible from adjacent roadways or public parking areas.
2. Large buildings should have facades that include variations in massing, form, and texture. Continuous surface treatments of a single material should be minimized. Architecture should be used to highlight building entries.
3. Any accessory buildings and enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.
4. Continuous arcades along the front of commercial buildings are encouraged as they provide the pedestrian protection from the weather, reduce solar gain, and can serve to enhance the character of what might otherwise be simple, formless structures. When more than one building is to be developed on a commercial site, the buildings should be designed to relate to one another as a total composition with well thought out relationships to one another.
- ~~5. Metal buildings shall only be allowed where the industrial nature of the use seems to mandate this type of construction. If metal buildings are found appropriate, surface treatment to the office portion of such structure facing the public street shall be required.~~

## Signs

1. Signs must conform to the requirements of Signs, Title 10, Article 35 of the Tracy Municipal Code as modified herein.
2. A site sign program ~~shall~~ should be integrated into a total design concept for a site and its buildings. The primary goal of the project sign system is to provide information and identification. When more than one sign is permitted, all signs shall be of similar style, shape, and materials.
3. All signs must be approved ~~as part of the Final Development Plan. A sign program shall be submitted as part of the application prior to installation, and should be designed in a manner that coordinates the sign designs and locations with the site plan and building architecture for each project.~~ The sign ~~program plans~~ should include:
  - a. **Detached Business Identification Signs:** One such monument sign (as defined by the Tracy Municipal Code) shall be allowed for each street frontage of the site. These signs may only contain the symbol and/or name of the business and its street address. The sign shall be free standing, may be double-sided, and shall be set back a minimum of 5 feet from the public right-of-way. Sign area shall not exceed 32 square feet per frontage and sign shall not exceed 6 feet in height from finished grade. Signs should generally be oriented perpendicular to approaching traffic.
  - b. **Wall signs:** On large single tenant buildings, signs should be located immediately above or adjacent to the primary building entrance. No sign shall extend above dominant roof lines. The area of any single sign shall not exceed 100 square feet. Total area shall not exceed one-half square foot of sign per lineal foot of business being served.

On smaller multi-tenant buildings, signs should be located at the frontage of each individual lessee. The area of any single sign shall not exceed 100 square feet nor more than 75 percent of the tenant frontage. Capital letters shall be no more than 2.5 feet in height and lower case letters no more than 1.5 feet in height. When individually-lettered wall signs comprise over 50 percent of the sign area of all sign types, total sign area shall not exceed 1.2 square feet per lineal foot of business being served. When comprising less than 50 percent of the total sign



area, the maximum sign area shall be one-half square foot per lineal foot of business being served.

- c. **Directional Signs:** Signs required or desired to assist patrons in accessing the facility shall be located in the site parking areas. The design of such signs shall be simple and easily legible. There is no limit to the number of signs provided on a site; however, no single sign shall exceed 6 square feet in area, except that vehicular “stop” signs shall be mounted per State standards.
- 4. A sign may be illuminated provided that no flashing, traveling, animated, or intermittent illumination shall be used. Such illumination shall be confined to the area of the sign except when such illumination is back lighting for an otherwise non-illuminated sign. No sign illumination shall cast a glare which is visible from any street.
- 5. Signs should be constructed with quality materials and in a craftsman-like manner to ensure both an attractive appearance and durability.

**Landscaping**

Minimum on-site landscaping requirements shall be established by Off-Street Parking Requirements (Title 10, Article 26 of the Tracy Municipal Code), except as modified below.

| <i>Summary of Requirements</i>              | <i>Commercial</i>   | <i>Industrial</i>    |
|---|---------------------|----------------------|
| Landscaped frontage setback                 | 10 feet             | 15 feet              |
| Minimum number of trees in parking area     | 1 tree per 5 spaces | 1 tree per 10 spaces |
| Percentage of landscaping in parking areas: |                     |                      |
| 0 - 15 cars                                 | 5%                  | 5%                   |
| 16 - 30 cars                                | 10%                 | 5%                   |
| 31-60 cars                                  | 15%                 | 7½%                  |
| Over 60 cars                                | 20%                 | 10%                  |

- 1. While commercial uses benefit from a well-landscaped parking area and visibility from the street, views of industrial uses benefit from a more generously landscaped streetscape. Thus, parking lot landscaping requirements for industrial uses may be reduced as specified in the Off-Street Parking Requirements in order to create a large landscape setback along the street. These provisions allow the reduction of 50 percent of the required landscaping based on the provision of a 15 foot landscape setback along the street frontage. The 15 foot strip may be included in the calculation of the total parking lot landscaping requirement. The remainder of the landscaping requirement must be distributed over the lot(s) to provide shade and landscape building frontage. Canopy trees shall be evenly distributed throughout the parking area to provide shade.
- 2. On-site landscaping along arterials-rights-of-way between property lines and buildings, parking lots, or vehicular circulation improvements shall be installed by the property owner. This landscaping shall be designed as an extension of the adjacent public arterial-right-of-way landscaping. Completion of landscaping on the site shall be simultaneous with completion of the building and other improvements on the site.
- 3. Landscaping shall not obstruct sight lines at street or driveway intersections.
- 4. In place of the wheel stops at parking lots, landscape areas and pedestrian walkways may be extended not more than 2 feet into required parking spaces, to include a 6” concrete curb. In such cases, no credit toward parking lot landscape requirements shall be given for the resulting additional landscaping.

5. Screening of the parking area from public rights-of-way in industrial areas shall be provided with a 2½ to 3 foot high element, measuring from the top of the parking area pavement. Screening may consist of one or a combination of the following:
  - a. Berms landscaped with ground cover, trees, and shrubs;
  - b. Solid, low profile, decorative masonry walls;
  - c. Evergreen shrubbery which, when solely used as screening, shall be continuously maintained to provide solid screening.
6. Generous landscaping screening is required adjacent on all street frontages for industrial areas. These areas should be landscaped with a combination of trees, shrubs, and ground cover to soften views of parking areas.
7. Tree planting and selection and massing should be compatible with streetscape plantings. Provide minimum 1 tree per 400 square feet of landscape setback. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together.
8. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs, and turf is encouraged. The use of turf in the narrow planting islands is discouraged.
9. Live plant materials shall be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole groundcover material.
10. All trees shall be of 24 inch box size minimum at planting with a minimum branching height 5 years after installation of 10 feet above road or parking surfaces and 6 feet at pedestrian areas. Shrubs shall be of 5 gallon size minimum with a maximum on-center spacing of 24 inches. Likewise, groundcover may be planted at 1 gallon size minimum with a maximum spacing of 12 inches on center.
11. Automatic irrigation is required for all landscaped areas. Irrigation systems ~~should~~ shall be designed so as not to overspray walks, buildings, and parking areas.

### Screening and Storage

1. All exterior trash areas, storage structures, and service areas ~~should~~ shall be screened from public view with a wall or fence of a minimum height of 8 feet above the street curb level. Storage areas shall be set back a minimum of 50 feet from streets, unless fully enclosed in an architecturally compatible enclosure.
2. No storage areas are allowed within the landscape easements, front setbacks, or side or rear yard landscaped buffers.
3. Roof-mounted equipment shall be screened from street view. Pad-mounted transformers, utility connections, and meter boxes shall be screened and integrated into the site plan.
4. The design of masonry walls, fencing, trash enclosures, and similar accessory site elements should be compatible with the architecture of the building and should use similar materials. Where masonry walls are along property frontage, they should enhance the entrance to the property and should not impair traffic safety by obscuring views. Long expanses of wall surfaces should be architecturally designed to prevent monotony.
- 4.5. The use of chain link fences shall be discouraged, and no chain link fences shall be visible from any public right-of-way.

### Development Review Process

All development in the NEI shall be subject to the requirements of Tracy Municipal Code Article 30, Development Review (TMC Sections 10.08.3920 through 10.08.4110).

## Environmental Performance Standards

### Use Restrictions

No use shall be permitted to exist or operate on any lot which:

1. Emits dust, sweepings, dirt, cinders, fumes, odors, radiation, gases and vapors, or discharges liquid or solid wastes or other harmful matter into the atmosphere or any body of water which may, according to the appropriate agency, adversely affect the health and safety persons within the area or the health and safety of persons in adjacent areas or the use of adjacent properties.
2. Discharges waste or any harmful substance, as defined by the Municipal Code, into any public sewer or storm drainage system.
3. Produces intense glare or heat, unless such use is performed only within an enclosed or screened area, and then only in such manner that glare or heat emitted will not be discernible from any exterior lot line.
4. Creates a sound pressure level in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
5. Allows the visible emissions of smoke (outside any building) other than the exhausts emitted by motor vehicles or other transportation facilities or any emissions in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
6. Creates a ground vibration that is perceptible, without instruments, at any point along any of the exterior lot lines.

### Hazardous Wastes and Water Pollutants

- ~~1. An on-site reconnaissance for hazardous wastes must be conducted for each parcel within the study area and the resulting report submitted with the application for the first proposed Tentative Map. If hazardous wastes are identified, they must be dealt with to the satisfaction of the Tracy Municipal Code before the application may be approved.~~
- ~~2.1.~~ All new industries locating with the area will be required to obtain a Discharge Permit from the Director of Utilities prior to occupancy. This permit shall establish the amount and quality of wastes allowed to be discharged into the City's sanitary sewer.
- ~~3.2.~~ The quality of wastewater entering the city sewage system from the proposed uses shall be measured by the Biochemical Oxygen Demand (BOD) and Total Suspended Solids (TSS) levels referenced in the local Water Quality Control Board 208 Plan. Users that are not expected to comply with these standards will be required to provide on-site pretreatment facilities.
- ~~4.3.~~ The storage and distribution of hazardous materials shall be subject to the rules of the San Joaquin County Health District.
- ~~5.4.~~ Industries regularly using significant quantities of hazardous chemicals as defined by State Law in the course of their operations shall be required to obtain a Conditional Use Permit.

### Mineral Extraction

1. Mineral extraction shall only be permitted as a subsidiary use to main use of the site.
2. Locate equipment, storage, and facilities for mineral extraction to avoid visibility from the public street.

### Dairy Uses

1. With development applications for an individual site, provide information demonstrating provisions of adequate buffers between proposed development and adjacent existing dairy uses.
2. Existing dairy uses are exempted from the Environmental Performance Standards.

**Permitted and Conditionally Permitted Uses**

The following table indicates uses which are to be permitted and permitted subject to ~~condition~~ Conditional Use Permits in the Northeast Industrial area. The land use designations are abbreviated as follows:

- LI: Light Industrial
- GC: General Commercial

The uses shown in Table 18 as Permitted (“P”) are those that are deemed acceptable anywhere in the assigned land use designation. They are uses that, when developed in conformance with this Plan, will not require special conditions in order to avoid negative impacts.

The uses shown as Conditionally Permitted (“C”) are of two types. Some of these uses are not acceptable in every location within a land use but are acceptable in certain locations. Other uses may require special conditions to make them acceptable at particular locations, due to their potential negative impacts on existing or planned uses. This may be because of their potential nuisance aspects, such as ~~litter,~~ noise, or hazardous wastes.

Where neither a “P” or “C” is shown for a particular land use district, that use is not allowed.

*Table 18: Permitted and Conditionally Permitted Land Uses ~~Industrial and Commercial Uses~~*

| <b>Land Uses</b>  | <b>LI ●</b> | <b>GC</b> |
|---|-------------|-----------|
| Agricultural, including dairies   | P ◊         | P ●, ◊    |
| Accessory uses and structures; not including warehouses located on the same site as a permitted use   | P           | P         |
| Administrative, executive, research, medical offices  | P           | P         |
| <u>Call centers</u>   | <u>P</u>    | <u>P</u>  |
| Accessory uses and structures located on the same site as a conditional use   | C           | C         |
| Warehousing and distribution facilities   | P           |           |
| Manufacturing, repair, assembly, or packaging of products from previously prepared materials, such as cloth, plastic, leather, or semi-precious metals or stones, but not including such operations as saw or planing mills, any manufacturing involving primary production of wood, metal, or chemical products from raw materials | P           |           |
| Manufacture of food products, pharmaceuticals, biotechnology products and the like, but not including fish or meat products, sauerkraut, vinegar, or the like, or rendering or refining of fats and oils.   | P           |           |
| Laboratories, including chemical, physical materials testing, electronic, agricultural, photographic film processing, and general research  | P           |           |

|   |   |   |
|---|---|---|
| Electrical industrial apparatus manufacturing, service, and repairs, including motors, generators, welding equipment, electrical transmission and distribution equipment, and turbines and pumps.   | P |   |
| Manufacture, repair of optical electronic, timing, and measuring instruments  | P |   |
| Dairy products plants   | P |   |
| Machine shops   | P |   |
| Heating, plumbing, and ventilating equipment manufacturing, servicing, repairs  | P |   |
| Refrigerator, furnace, water heater, and other household appliance manufacturing, service and repairs, not incidental to retail sales   | P |   |
| Furniture and cabinet assembling whose activities are carried on entirely within an enclosed building and which have no construction yards on the lot   | P |   |
| Parcel delivery service and vehicle storage inside and outside the building   | P |   |
| Truck terminals   | P |   |
| Mini storage  | P |   |
| Equipment storage   | P |   |
| Janitorial services and supplies  | P |   |
| Printing, including lithographing, engraving, and other such similar reproduction services  | P |   |
| Automotive supply stores  | C |   |
| Rental yards, including the rental of hand tools, garden tools, power tools, trucks, trailers, and other similar equipment  | C |   |
| Building materials sales, lumberyards (outside storage)   | C |   |
| Repair, painting, and body work for automotive, motorcycle, and farm machinery  | C |   |
| Boat sales, service, repair   | C |   |
| Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, and retail products typically found in a convenience store. | C |   |
| Wholesale trade business  | C |   |
| Intermediate manufacturing uses involving the processing of raw materials, including food and paper processing, wineries, and concrete mixers   | C |   |
| Mineral and hydrocarbon extraction  | C |   |
| Recycling (collection and sorting)  | C |   |
| Outlet stores and centers   |   | P |
| Warehouse retail, including furniture, office supply, sporting goods, or wholesale merchandising  |   | P |
| Hardware stores, including garden centers   |   | P |

|   |  |          |
|---|--|----------|
| Home improvement and interior decorating stores, including carpet, drapery, floor covering, paint, glass, and wallpaper shops   |  | P        |
| Garden centers, including plant nurseries and retail sales  |  | P        |
| Sporting goods and toy stores   |  | P        |
| Specialty retail stores selling those items and services normally sold in department stores (including clothing, shoes, and accessories)  |  | P        |
| Shoe and clothing repair  |  | P        |
| Food markets, convenience markets   |  | P        |
| Liquor stores   |  | P        |
| Drug stores and prescription pharmacies   |  | P        |
| Restaurants, including fast food  |  | P        |
| Art galleries and artists supply stores   |  | P        |
| Barbershops, beauty shops, and hairstylists   |  | P        |
| Financial institutions, including banks, savings and loan offices, finance companies, credit unions, and related services   |  | P        |
| Refrigerator, furnace, water heater, and other household appliance sales including repair, provided repair services are incidental to retail sales.   |  | P        |
| Laundries and dry cleaners  |  | P        |
| Photocopying and related duplicating services, not including printing, lithographing, engraving, or such similar reproduction services  |  | P        |
| Pet and bird stores with incidental veterinary services   |  | P        |
| <u>Animal Shelters</u>  |  | <u>P</u> |
| Video stores, sales, and rental   |  | P        |
| Automobile and motorcycle sales and rental, including new and used sales  |  | C        |
| Bars without significant food service   |  | C        |
| Building materials sales (no outside storage)   |  | C        |
| Miniature golf  |  | C        |
| Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, and retail products typically found in a convenience store. |  | C        |

●: Subject to compliance with the Environmental Performance Standards. Note that as defined by State law, any business using significant quantities of hazardous materials requires a Conditional Use Permit.

◊: Existing dairy operations are exempt from the Environmental Performance Standards.

RESOLUTION 2012-\_\_\_\_\_

RECOMMENDING THAT THE CITY COUNCIL APPROVE AN ORDINANCE ADOPTING THE NEI SPECIFIC PLAN, AMENDING THE TRACY MUNICIPAL CODE SECTION 10.08.980 (NAMES OF ZONES)AND ADDING NEW SECTION 10.08.3021 (NEW ARTICLE 22.5, NORTHEAST INDUSTRIAL SPECIFIC PLAN) AND REZONING THE NORTHEAST INDUSTRIAL SPECIFIC PLAN AREA FROM PLANNED UNIT DEVELOPMENT TO NEI SPECIFIC PLAN APPLICANT IS THE CITYOF TRACY – APPLICATION NUMBERS SPA12-0003, ZA12-0006 AND R12-0003

WHEREAS, the NEI Concept Development Plan was approved by City Council on May 8, 1996 (Resolution Number 96-146), and was amended by Resolution Numbers 99-106, 99-107, 2005-091, and 2008-046 , and

WHEREAS, the NEI EIR was certified by the City Council on May 8, 1996 (Resolution Number 96-144), and

WHEREAS, by converting the essential elements of the NEI Concept Development Plan into a Specific Plan under Tracy Municipal Code Chapter 10.20 and adding a reference to that Specific Plan within the City’s zoning ordinance, the evaluations of development applications within the NEI planning area will be simpler and more streamlined, and,

WHEREAS, on June 27, 2012 the Planning Commission held a public hearing to review and discuss the proposed NEI Specific Plan, zone text amendment to establish an NEI Specific Plan Zone, and the rezoning of NEI from PUD to NEI Specific Plan;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission hereby recommends that the City Council approve the NEI Specific Plan, zone text amendment to establish an NEI Specific Plan Zone, and the rezoning of NEI from PUD to NEI Specific Plan as indicated in Exhibit 1.

\* \* \* \* \*

The foregoing Resolution 2012-\_\_\_\_\_ was adopted by the Planning Commission on the 27<sup>th</sup> day of June, 2012, by the following vote:

AYES: Commission Members:  
NOES: Commission Members:  
ABSENT: Commission Members:  
ABSTAIN: Commission Members:

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
Staff Liaison

ORDINANCE \_\_\_\_\_

AN ORDINANCE OF THE CITY OF TRACY ADOPTING THE NORTHEAST INDUSTRIAL SPECIFIC PLAN, AMENDING SECTION 10.08.980, NAMES OF ZONES, AND ADDING SECTIONS 10.08.3022, NORTHEAST INDUSTRIAL SPECIFIC PLAN ZONE AND 10.08.3023, DEVELOPMENT REVIEW (NORTHEAST INDUSTRIAL SPECIFIC PLAN) TO THE TRACY MUNICIPAL CODE

WHEREAS, the Northeast Industrial (NEI) Concept Development Plan, a Planned Unit Development (PUD) was adopted by the City Council on May 8, 1996 (Resolution Number 96-146). It was amended by Resolution Nos. 99-107, 99-107, 2005-091 and 2008-046.

WHEREAS, the procedure for even a simple modification to a PUD Concept Development Plan is unnecessarily burdensome, expensive and time consuming to applicants because it requires a zoning ordinance amendment to the PUD, and

WHEREAS, by converting the essential elements of the Northeast Industrial Area Concept Development Plan into a Specific Plan (under Tracy Municipal Code Chapter 10.20) and adding a reference to that Specific Plan in the City's zoning ordinance, the handling of applications within the area will be much simpler and more straightforward.

WHEREAS, the Planning Commission held a public hearing to review and discuss the proposed NEI Specific Plan, zone text amendment and rezone from PUD to NEI Specific Plan on June 27, 2012, and

WHEREAS, the City Council held a public hearing to review and discuss the proposed NEI Specific Plan, zone text amendment and rezone from PUD to NEI Specific Plan on July 17, 2012, and

WHEREAS, the NEI Environment Impact Report was certified by the Tracy City Council on May 8, 1996 (Resolution Number 96-144) and the proposed NEI Specific Plan is consistent with that EIR;

The Tracy City Council hereby ordains as follows:

**SECTION 1:** The Northeast Industrial Area Specific Plan, dated July 17, 2012, is adopted. This Specific Plan supersedes the former Northeast Industrial Areas Concept Development Plan, which is now repealed. References elsewhere in City documents, such as the Finance Implementation Plan and development impact fee accounts are changed to now refer to the Northeast Industrial Area Specific Plan.

**SECTION 2:** Section 10.08.980, Names of zones, of the Tracy Municipal Code, is amended to read as follows:

**"10.08.980 - Names of zones.**

In order to classify, regulate, restrict, and segregate the uses of land and buildings, to regulate and restrict the height and bulk of buildings, to regulate the area of yards and other open spaces about buildings, and to regulate the density of population, the following zones are hereby established:

- (a) Residential Estate Zone .....RE;



- (b) Low Density Residential Zone .....LDR;
- (c) Medium Density Cluster Zone .....MDC;
- (d) Medium Density Residential Zone .....MDR;
- (e) High Density Residential Zone .....HDR;
- (f) Medical Office Zone .....MO;
- (g) Professional Office and Medical Zone .....POM;
- (h) Planned Unit Development Zone .....PUD;
- (i) Residential Mobile Home Zone .....RMH;
- (j) Community Shopping Center Zone .....CS;
- (k) Neighborhood Shopping Zone .....NS;
- (l) Central Business District Zone .....CBD;
- (m) General Highway Commercial Zone .....GHC;
- (n) Light Industrial Zone .....M-1;
- (o) Heavy Industrial Zone .....M-2;
- (p) Highway Service Zone .....HS;
- (q) Agricultural Zone .....A; ~~and~~
- (r) Airport Overlay Zone .....AO; ~~and-~~
- (s) Northeast Industrial Area Specific Plan Zone ...NEI."

SECTION 3: A new Article 22.5, Former PUD areas, and a new Section 10.08.3022, Northeast Industrial Area Specific Plan Zone, and a new section 10.08.3023, Development review (Northeast Industrial Specific Plan) are added to the Tracy Municipal Code to read as follows:

**“Article 22.5 Former PUD Areas**

**10.08.3022 Northeast Industrial Specific Plan Zone.**

The zoning within the Northeast Industrial Specific Plan Zone is governed by the Northeast Industrial Area Specific Plan.”

**10.08.3023 Development review (Northeast Industrial Specific Plan).**

All uses requiring a building permit shall obtain development review compliance, except as provided in Article 30 of this chapter and the CEQA Guidelines adopted by the City, prior to being established in the Northeast Industrial Specific Plan Zone.

SECTION 4. This Ordinance shall take effect thirty (30) days after its final passage and adoption.

SECTION 5. This Ordinance shall be published once in the Tracy Press, a newspaper of general circulation, within fifteen (15) days from and after its final passage and adoption.

\* \* \* \* \*

The foregoing Ordinance \_\_\_\_\_ was introduced at a regular meeting of the Tracy City Council on the 17th day of July, 2012, and finally adopted on the 7th day of August, 2012, by the following vote:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ATTEST:

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Mayor

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City Clerk

City of Tracy  
**NORTHEAST INDUSTRIAL  
SPECIFIC PLAN**

July 17, 2012

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## INTRODUCTION

The 870 acre Northeast Industrial Area Specific Plan aims to develop a well-planned industrial zone that will attract businesses to Tracy, and provide local employment opportunities. The Specific Plan anticipates a mixture of industrial uses, including rail-dependent industries and "flex-tech" light industrial.

## PLANNING AREA LOCATION

### Location

The Northeast Industrial planning area lies along the northeast boundary of the City of Tracy. The area is generally bounded to the north by 1-205, to the south by the Southern Pacific Railroad tracks, to the east by Banta Road, and the west by MacArthur Drive. Grant Line Road bisects the area.

### Surrounding Land Uses

Presently, properties along MacArthur Drive directly west of the site are developed with industrial uses, such as the U.S. Cold Storage facility. North of the site are industrial and commercial uses, including the Tracy Outlets at MacArthur Drive and Pescadero Avenue. The Yellow Freight Company is also located to the north, between the site and 1-205. Agricultural uses are found on lands to the east.

Lands directly to the north and west of the site are included in the 1-205 Corridor Specific Plan and the 1988 Industrial Areas Specific Plan. To the east is the residential Banta area of the county. The project site in the context of the existing industrial and commercial uses, and existing specific plan areas is shown in Figure 1.

### Land Ownership

The Northeast Industrial planning area is currently made up of 61 properties, with a variety of owners. Parcels range in size from small half acre home sites to large agricultural and industrial holdings. The assessor's parcel numbers and ownership of the area is shown in Figure 2.

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## EXISTING PLANNING AREA CONDITIONS

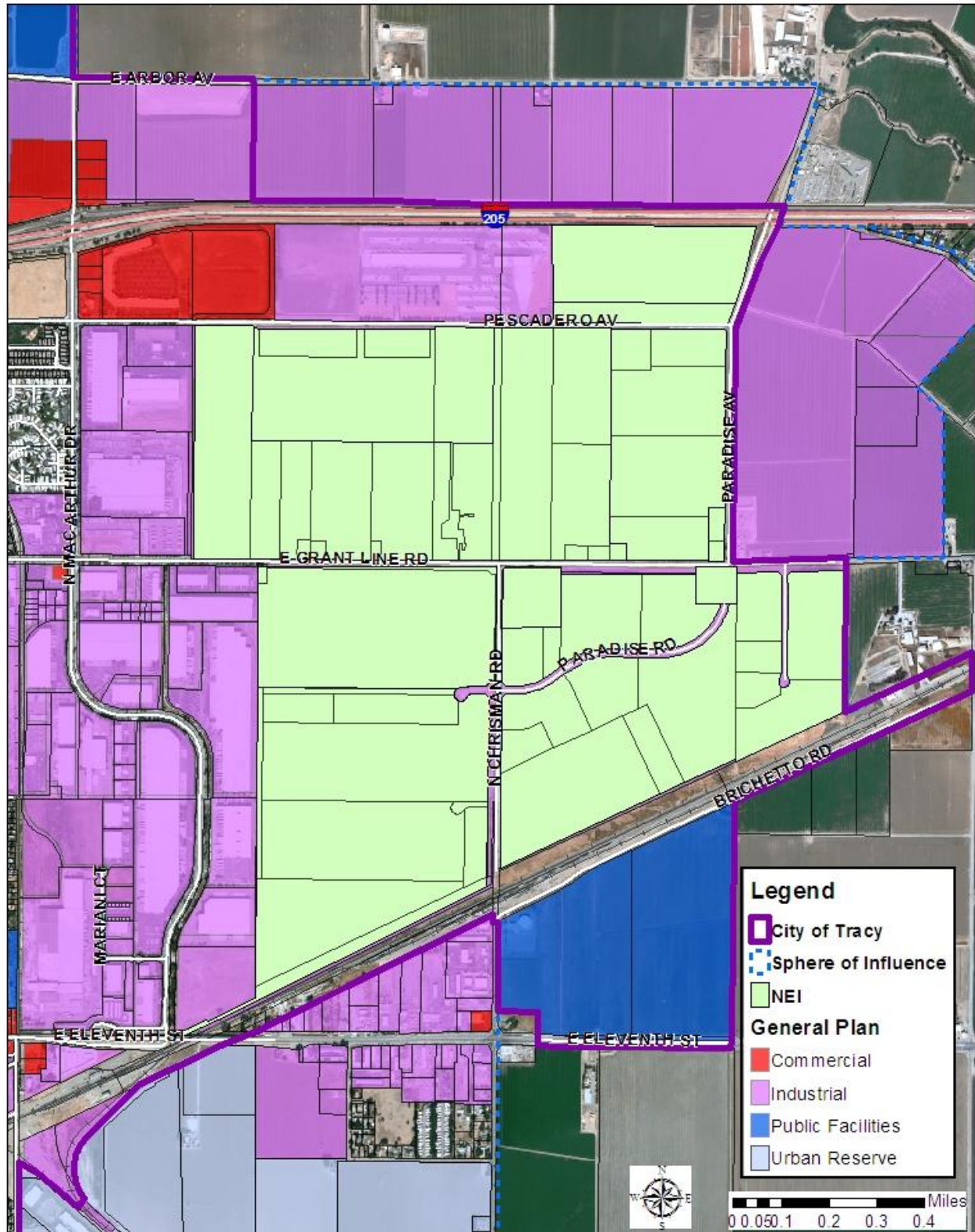
### Site Features

Land uses within the Northeast Industrial Area are currently light industrial and agricultural, with a number of dairy operations and rural residences remaining. Few other significant site features are present. The topography is relatively level.

A Westside Irrigation District (WSID) supply or tailwater ditch crosses the area. One property along 1-205 is located just outside the FEMA 100 year flood line found in the vicinity of the I-205/Paradise Road overcrossing.

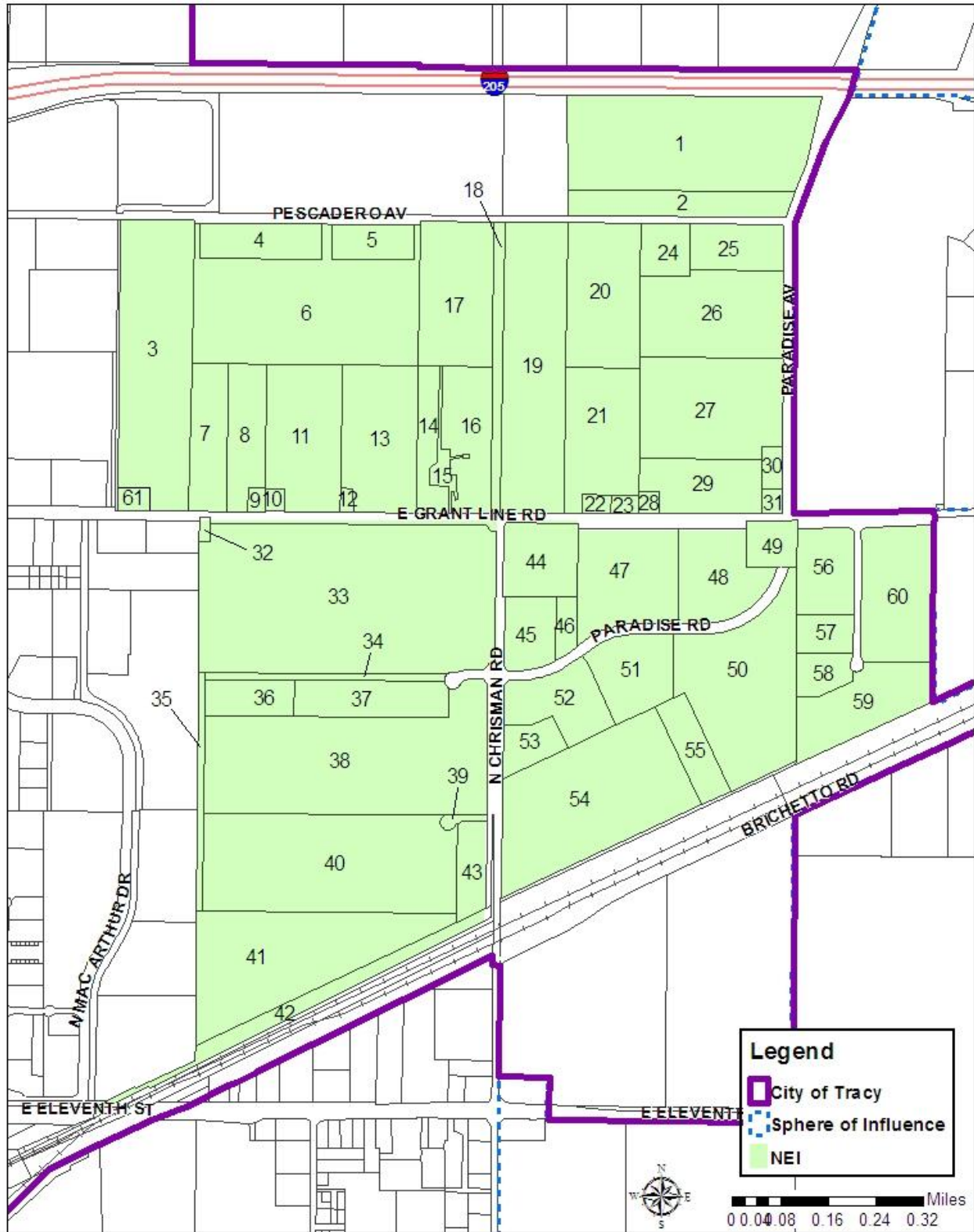
The existing conditions of the planning area are shown on Figures 3 and 4. None of the parcels are under Williamson Act contracts

# Surrounding Land Use Designations Figure 1



# Land Owners

Figure 2





| Number | APN        | OWNER                               | LOT SIZE (ACRES) |
|--------|------------|-------------------------------------|------------------|
| 1      | 213-060-12 | RADOS PROPERTIES CALIF LAND LLC     | 42.42            |
| 2      | 213-060-13 | HALEY DOROTHY TRUSTEE               | 9.78             |
| 3      | 213-070-81 | PIMENTEL VELMA C                    | 35.95            |
| 4      | 213-070-75 | PONY UP TRACY LLC                   | 7.68             |
| 5      | 213-070-74 | OLD GOLDEN OAKS LLC                 | 5.35             |
| 6      | 213-070-73 | US INDUSTRIAL REIT CONTAINER III    | 44.03            |
| 7      | 213-070-22 | PIMENTEL VELMA C                    | 9.79             |
| 8      | 213-070-28 | MATTSON HELENE A                    | 9.1              |
| 9      | 213-070-29 | MATTSON HELENE A                    | 0.689            |
| 10     | 213-070-19 | ROBERTSON STANLEY & B               | 0.703            |
| 11     | 213-070-20 | SILVA BERNARDINE M                  | 18.88            |
| 12     | 213-070-18 | ENDER WENDELL F & M L               | 0.519            |
| 13     | 213-070-40 | PIMENTEL JOSEPH L                   | 18.99            |
| 14     | 213-070-41 | MARTY MARILYN                       | 6.32             |
| 15     | 213-070-17 | MARTY MARILYN                       | 2.27             |
| 16     | 213-070-39 | MARTY MARILYN                       | 11.41            |
| 17     | 213-070-06 | TRACY PESCADERO INDUSTRIAL PARK     | 19.46            |
| 18     | 213-070-48 | SILVA MARIA O                       | 5.34             |
| 19     | 213-070-49 | SILVA MARIA O                       | 33.86            |
| 20     | 213-070-08 | SILVA MARIA O                       | 19.55            |
| 21     | 213-070-51 | SILVA MARIA O                       | 17.55            |
| 22     | 213-070-52 | SILVA MARIA O                       | 1.005            |
| 23     | 213-070-53 | SILVA MANUEL H                      | 1.005            |
| 24     | 213-070-76 | PROLOGIS LOGISTICS SERVICES INC     | 4.8              |
| 25     | 213-070-77 | PROLOGIS LOGISTICS SERVICES INC     | 8.19             |
| 26     | 213-070-78 | PROLOGIS LOGISTICS SERVICES INC     | 22.08            |
| 27     | 213-070-79 | PROLOGIS LOGISTICS SERVICES INC     | 25.15            |
| 28     | 213-070-13 | ENDER ALVETA F                      | 0.839            |
| 29     | 213-070-80 | PROLOGIS LOGISTICS SERVICES INC     | 11.3             |
| 30     | 213-070-44 | RUSE JOSEPH T & PATRICIA A          | 1.619            |
| 31     | 213-070-45 | ENDER BUDDY C                       | 0.9              |
| 32     | 250-020-15 | COSTA ROBERT J SR & EVELYN M        | 0.5              |
| 33     | 250-020-14 | ROCHA MANUEL TOSTE JR               | 79.03            |
| 34     | 250-020-79 | CATELLUS CORPORATE CTR TRACY LLC    | 3.17             |
| 35     | 250-020-82 | CATELLUS CORPORATE CTR TRACY LLC    | 2.87             |
| 36     | 250-020-80 | CATELLUS CORPORATE CTR TRACY LLC    | 5.8              |
| 37     | 250-020-81 | CATELLUS CORPORATE CTR TRACY LLC    | 10.08            |
| 38     | 250-020-83 | CATELLUS CORPORATE CTR TRACY LLC    | 52.13            |
| 39     | 250-020-84 | CATELLUS CORPORATE CTR TRACY LLC    | 0.773            |
| 40     | 250-020-85 | CATELLUS CORPORATE CTR TRACY LLC    | 44.44            |
| 41     | 250-020-87 | CATELLUS CORPORATE CTR TRACY LLC    | 33.53            |
| 42     | 250-010-05 | UNION PACIFIC RAILROAD COMPANY      | 9.18             |
| 43     | 250-020-86 | CATELLUS CORPORATE CTR TRACY LLC    | 5                |
| 44     | 250-030-10 | SILVA FRANK I & MARY L              | 9.28             |
| 45     | 250-030-29 | 1851 E PARADISE ROAD PARTNERS LLC   | 6.56             |
| 46     | 250-030-30 | BIG 4 GROUP LLC                     | 2.17             |
| 47     | 250-030-27 | BARBOSA INVESTMENT GROUP LTD PTP    | 17.73            |
| 48     | 250-030-28 | BARBOSA INVESTMENT GROUP LTD PTP    | 12.59            |
| 49     | 250-030-02 | TRACY CITY OF                       | 4.24             |
| 50     | 250-030-26 | HEADLANDS REALTY CORP               | 31.17            |
| 51     | 250-030-25 | HEADLANDS REALTY CORP               | 10.51            |
| 52     | 250-030-24 | TRACY LOGISTICS CENTER PARTNERS LLC | 10.48            |
| 53     | 250-030-23 | TRACY LOGISTICS CENTER PARTNERS LLC | 4.48             |
| 54     | 250-030-18 | TCE TRACY LLC                       | 37.96            |
| 55     | 250-030-19 | AMB HOLDCO LLC                      | 6.35             |
| 56     | 250-280-09 | AMB PROPERTY LP                     | 8.81             |
| 57     | 250-280-08 | AMB PROPERTY LP                     | 3.96             |
| 58     | 250-280-07 | AMB PROPERTY LP                     | 3.89             |
| 59     | 250-280-06 | AMB PROPERTY LP                     | 14.14            |
| 60     | 250-280-10 | AMB PROPERTY LP                     | 17.8             |
| 61     | 213-070-82 | CITY OF TRACY                       | 1.089            |

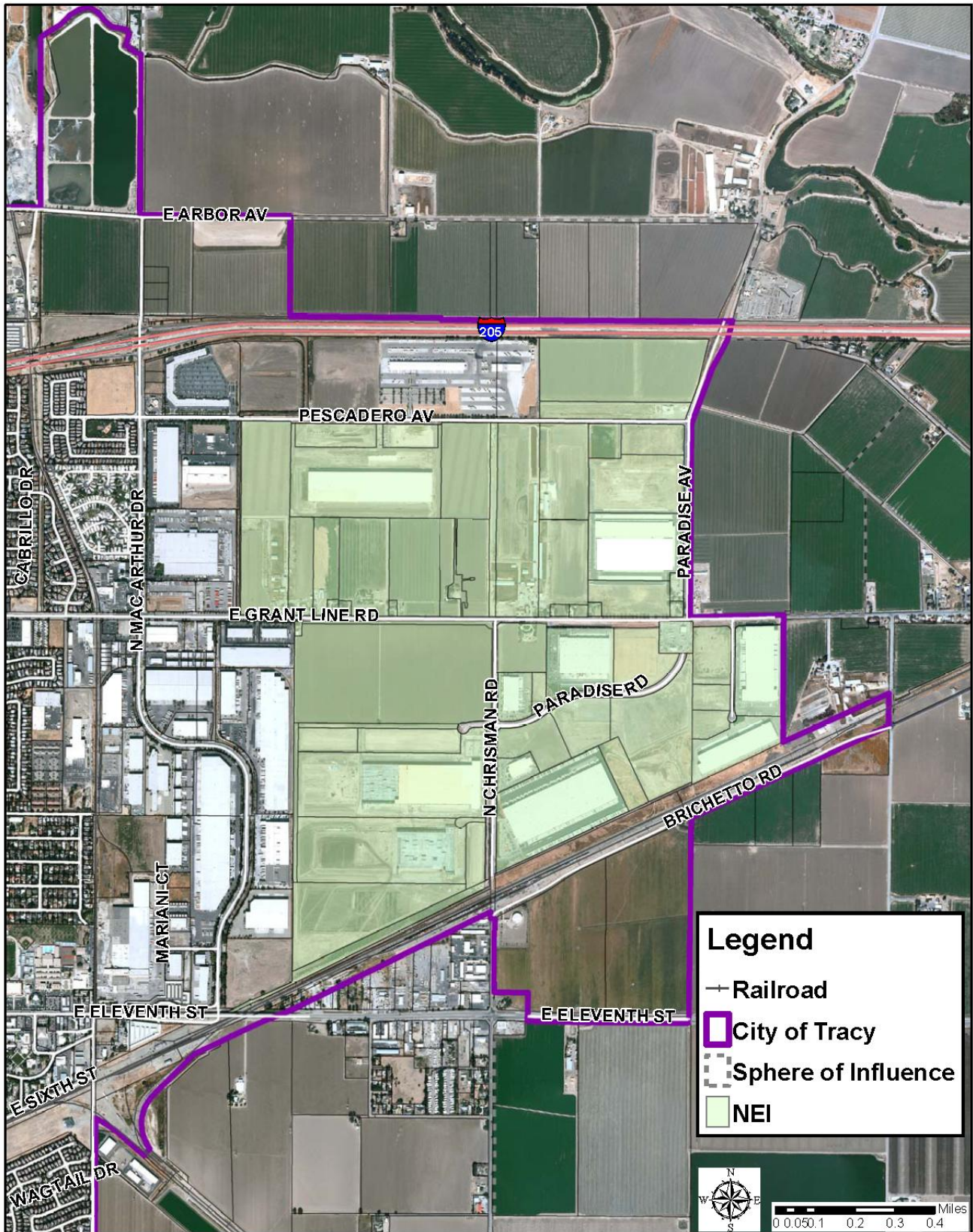
**Existing Roadway Network**

The Plan area is served by a number of existing roadways, some of which have been improved to meet the circulation needs of the NEI area, and others that will be constructed or improved as traffic demand requires.

Regional access for the area is provided by Interstate 205 (I-205) to the north and Interstate 5 (I-5) to the east. I-205 is an east-west freeway which extends from I-5 west to I-580 with I-580 continuing west into the Bay Area. I-5 is a north-south freeway which extends throughout California. Currently, Plan area access to I-205 is via an interchange at MacArthur Drive. I-5 access is via the full access interchange at Kasson Road (an extension of Grant Line Road). I-5 access is also available (to/from the north only) via an interchange at Eleventh Street. The primary local roadways serving the area are MacArthur Drive, Pescadero Avenue, East Grant Line Road, Chrisman Road and Paradise Road

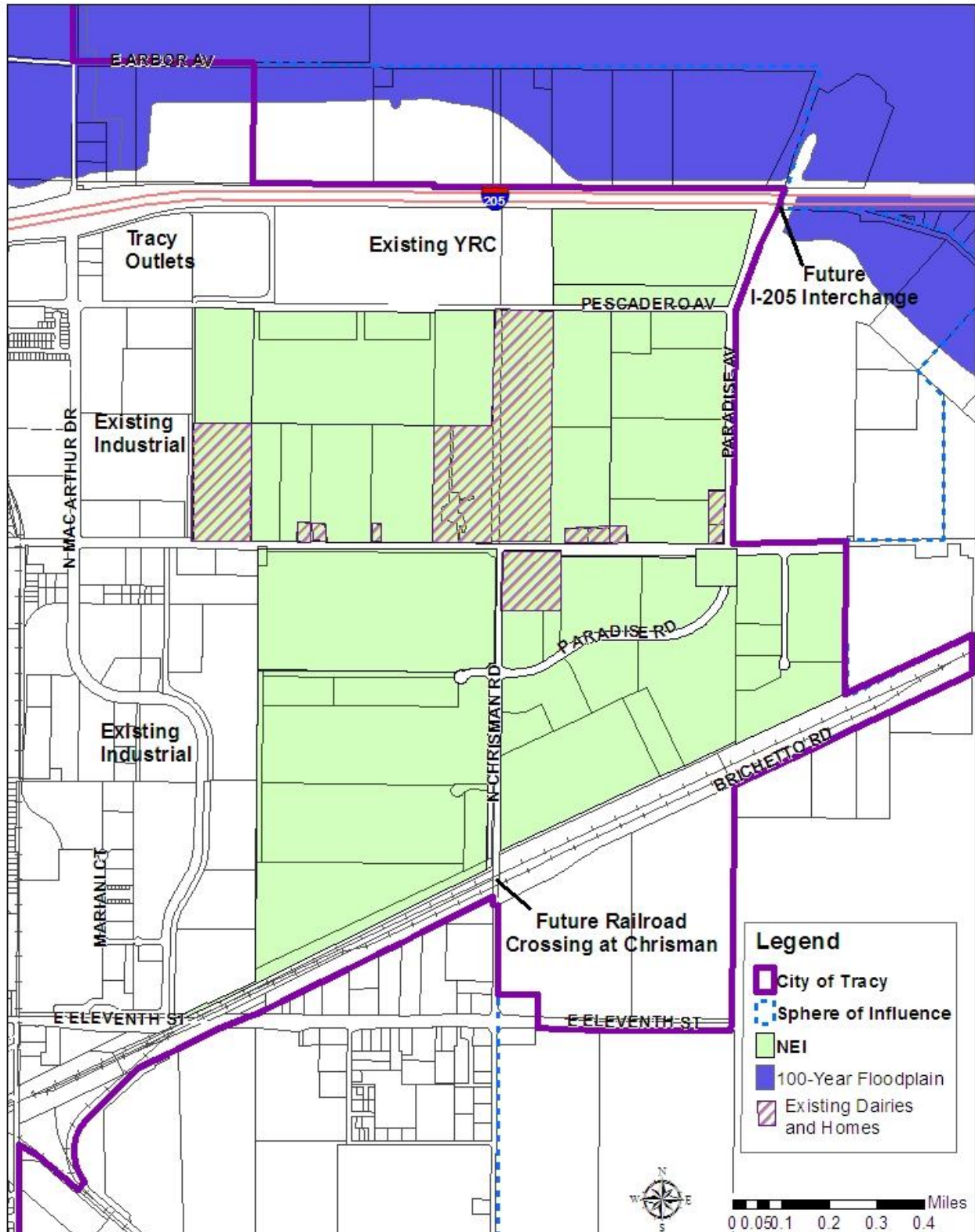
# Aerial

Figure 3



# Existing Site Conditions

Figure 4



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## RELATIONSHIP TO TRACY'S GENERAL PLAN

### General Plan Land Use Designations

The City of Tracy General Plan designates the entire Northeast Industrial area for Industrial land uses. Specific uses allowed in the industrial category range from flex/office space to manufacturing to warehousing and distribution. Ancillary uses, such as restaurants, consumer services, and parks may be allowed to serve the daily needs of the workers.

### Tracy's Growth Strategy

The City of Tracy has embraced a "balanced growth" strategy, seeking to direct growth in an efficient, cost-effective manner, balancing land uses and appropriate use of the land with well-planned and utilized infrastructure. This strategy seeks to improve the jobs/housing balance and to encourage development of employment opportunities and capitalize on freeway interchanges with industrial, retail, and service-related development.

The northeastern sector of Tracy has been designated as one area in which the City's industrial growth will occur. Adjacent to existing industrial development and with direct access to the I-205 freeway and rail transportation, the Northeast Industrial area is ideally situated to attract and support business without the need for major infrastructure expenditures.

### General Plan Goals Furthered by the Northeast Industrial Area Development

The Northeast Industrial Specific Plan proposes development consistent with the General Plan. It furthers many of the Goals and Policies stated in the General Plan and it implements many of the Actions set forth in the General Plan. Following is a summary of General Plan Goals, Policies, and Actions to which the Northeast Industrial Specific Plan contributes.

### Land Use

#### **Objective LU-1.1      Establish a clearly defined urban form and city structure.**

Policy P1: New development and redevelopment in existing areas shall be organized as a series of residential Neighborhoods, Employment Areas, Corridors, Village Centers, the Downtown and the I-205 Regional Commercial Area.

- Employment Areas are the job-centers of the city and include office districts, retail centers and industrial areas.

#### **Objective LU-2.3      Expand the City's industrial base.**

Policy P1: The Northeast Industrial Area should contain a mix of heavy industrial, light industrial, warehouse, and distribution users to maximize rail and highway access on large parcels of land. The Northeast Industrial Area should also contain commercial uses and services to meet the daily needs of workers.

#### **Objective LU-6.2      Ensure land use patterns that minimize conflicts between transportation corridors and neighboring uses.**

Policy P1: Uses that are compatible with the noise, air quality and traffic impacts associated with freeways, such as auto-oriented commercial and industrial uses, should be located near and along freeway corridors whenever possible.

## **Economic Development**

### **Objective ED-4.1 Ensure an adequate, balanced supply of all land uses for future economic development.**

Action A1: Monitor current and future land supply needs for industrial, office and retail growth.

### **Objective ED-6.3 Promote expansion in the Northeast Industrial Area.**

Policy P1: The City shall encourage and facilitate the development and buildout of the entire 870-acre Northeast Industrial Area.

Policy P2: The City shall direct business attraction efforts to manufacturing uses, rather than warehouse distribution facilities, due to their higher employment densities.

Policy P3: Developers should consider flexible facility design and construction types that can accommodate future manufacturing uses with higher employment densities.

Policy P4: The City should support efforts to attract private developers and equity investors to participate in the development of the area.

## **Community Character**

### **Objective CC-11.2 Encourage attractive design in Employment Areas.**

Policy P1: Development in Employment Areas should adhere to high-quality design standards.

Policy P4: Building setbacks for office buildings or office portions of industrial buildings should be minimized to ensure that buildings define the edges of the street.

Policy P6: Loading facilities in Employment Areas should be screened from view from public streets to the extent possible.

Policy P9: Fencing visible from the public right-of-way shall be visually appealing when used in industrial and commercial developments.

### **Objective CC-11.3 Minimize the impact of parking on the pedestrian environment in Employment Areas.**

Policy P1: The impact of parking in Employment Areas on the pedestrian environment should be minimized with attractive landscaping.

Policy P2: Parking lots should be set back from the street with a landscaped buffer wherever possible.

## **Circulation**

### **Objective CIR-1.4 Protect residential areas from commercial truck traffic.**

Policy P1: Significant new truck traffic generating uses shall be limited to locations along designated truck routes, in industrial areas or within ¼-mile of freeways.

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## PROJECT CHARACTERISTICS

### Project Objectives

The objectives of the project are:

- To develop the Northeast Industrial Area as a high-quality industrial and commercial site of significant benefit to the City of Tracy and the nearby region.
- To develop a well-planned site that will attract businesses to Tracy, providing local employment opportunities.
- To develop the Northeast Industrial Area for primarily mixed industrial uses, including rail-dependent industries.
- To minimize project-related impacts to Tracy's transportation network.
- To provide a flexible phasing program that allows market forces to dictate reasonable growth increments, while ensuring that agricultural properties are allowed to remain until ready to develop.
- To create a project consistent with the goals of the General Plan.
- To integrate the Northeast Industrial Area into the development pattern of the City of Tracy.
- To integrate mitigation for environmental impacts into the design of the project.

### Land Use Designations

The Northeast Industrial Area will consist primarily of light industrial land uses. In addition, general commercial land uses are planned at major intersections along Pescadero Avenue and Grant Line Road. Figure 5 shows the configuration of the land uses in the planning area.

The development prototypes included in the Design Guidelines indicate possible site planning scenarios for environmental impact analysis.

### Light Industrial Land Uses (LI)

Light Industrial land uses cover approximately 800 acres of the planning area. Assuming a Floor Area Ratio (FAR) of 0.5, approximately 17.5 million square feet of light industrial, warehouse and manufacturing facilities would be provided. Light industrial land uses would be compatible with existing industrial land uses to the west and north, as well as with freeway noise, and rail noise and vibration.

Several types of light industrial land uses are appropriate in the Northeast Industrial Area. It is anticipated that warehousing and distribution businesses with low employee densities will be the predominant development type. This development pattern is similar to those that have located in Tracy in recent years. The southern portion of the Planning Area is appropriate to uses that require rail access.

The City of Tracy is also interested in attracting higher employee density businesses to the area. It is anticipated that there may be a future demand for a "Flex-Tech" development that would accommodate research & development businesses and call centers.

The light industrial zone may also be appropriate for service commercial businesses with little pedestrian traffic, that are not necessarily compatible with general commercial land uses, such as automotive supply and plumbing stores. Commercial development may be permitted on sites designated Light Industrial without the site being redesignated General Commercial if:

1. The site is a minimum of one acre and located on the northwest or southwest corners of Pescadero Avenue and Paradise Road or the northwest or southwest corners of Grant Line Road and Paradise Road.
2. The site has City Engineer-approved access from at least two streets.
3. The proposal is for a convenience retail or general commercial service use to the industrial area, such as restaurants, hotels, or convenience stores and service stations.
4. 15,000 square feet of retail within an industrial warehouse located on the southwest corner of Chrisman Road and Paradise Road.

A variety of parcel sizes are foreseeable, probably ranging from 5 to 50 acres, depending upon the type of industrial user attracted to the area. Since market forces will dictate that eventual parcel size, the Specific Plan does not reflect an exact parcelization plan.

### **Existing Agricultural and Residential Uses**

The parcels containing existing dairy operations and residences, shown on the Land Use Map, that may remain for a number of years until ready to develop. The planned light industrial land uses will be compatible with these properties. (The agricultural lands are subject to the City's Right to Farm Ordinance, discussed below)

### **General Commercial Land Uses (GC)**

Three parcels of general commercial, totaling 45.5 acres are provided within the planning area. These commercial sites are intended to expand upon the existing commercial uses adjacent to the I-205/ MacArthur Drive interchange, take advantage of the freeway-oriented traffic circulation of the site, and serve the local industrial community. The largest parcel (25.5 acres) is located in the northwest corner of the project along Pescadero Avenue. This parcel provides for potential factory outlet expansion or other interstate-related commercial uses. General Commercial land uses provide for service commercial oriented land uses, such as animal shelters, who's land use characteristics are compatible with industrial uses.

### **Right to Farm Provisions**

A number of dairy operations currently exist within the Northeast Industrial planning area. Dairy facilities typically involve significant capital improvements. This Specific Plan intends to allow the continued operation of these existing dairy facilities and other agricultural uses until such time as the owners wish to convert their property to nonagricultural uses.

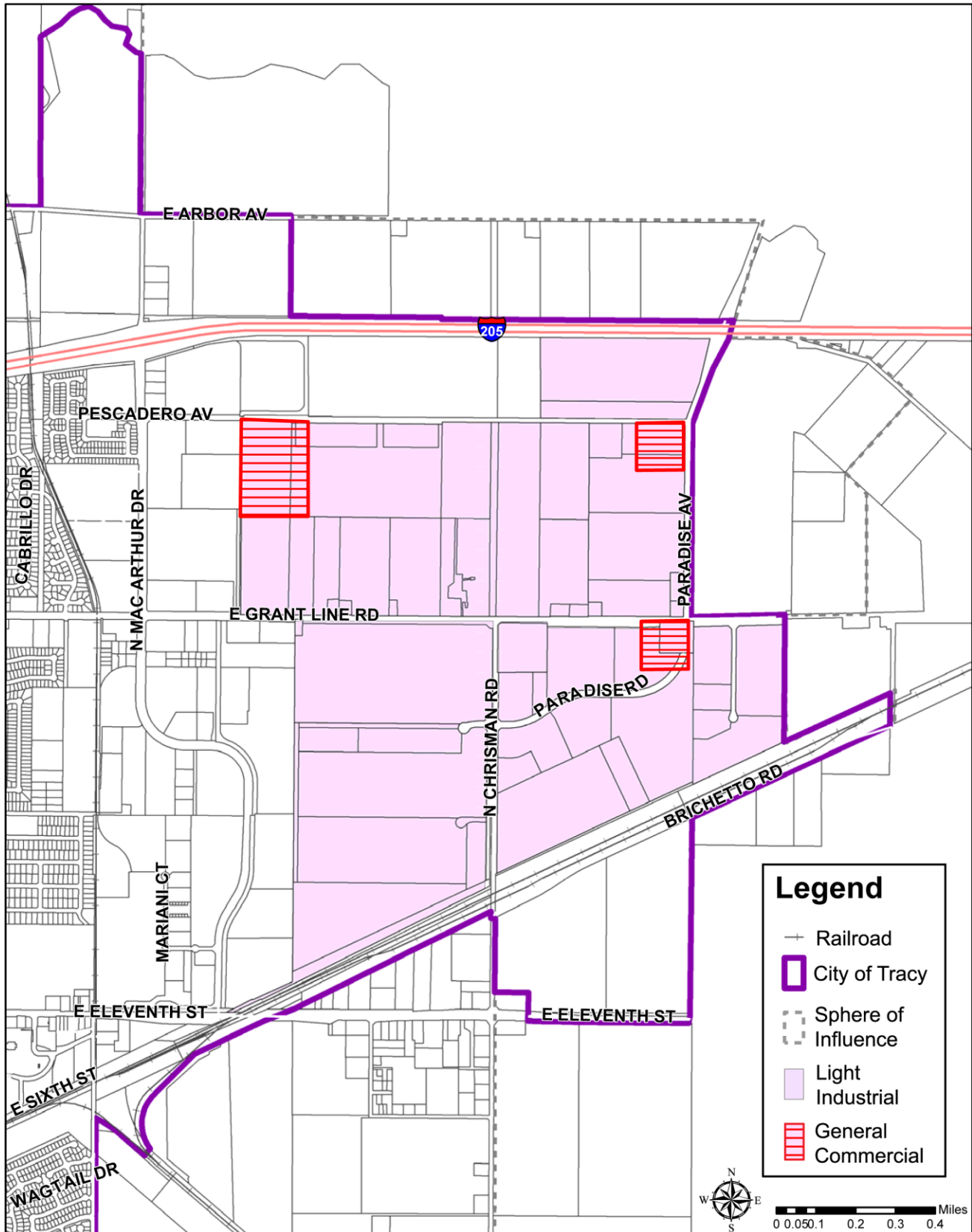
When nonagricultural land uses move into areas near to pre-existing agricultural operations, the agricultural operations frequently become the subject of nuisance complaints. Chapter 10.24 of the Tracy Municipal Code, the Right to Farm Ordinance, declares that farming operations are not a nuisance, and recognizes the right to farm within the incorporated City. Agricultural and dairy operations are included in the Permitted Uses (page \_\_) for both the Light Industrial and General Commercial land use designations.

### **Mineral Extraction**

A number of the parcels in the Northeast Industrial planning area have mineral rights that are owned by different owners than the underlying land. It is possible that these owners may choose to exercise these rights at some time in the future. As such, mineral extraction is included as a Conditionally Permitted use within the Light Industrial land use designation. Any mineral extraction operation will be unobtrusive, and will be a subsidiary use to the primary use of the parcel.



# Proposed Land Uses



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## **CIRCULATION AND TRANSPORTATION**

The Northeast Industrial Specific Plan provides for efficient circulation by automobiles and trucks. The proposed land use mix, street geometry, and proximity to the interstate freeway system will minimize project-related impacts to Tracy's transportation network.

The distribution, location and extent of the roadway improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future roadway improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figures 6 and 7A and 7B show the original roadway network and street sections for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

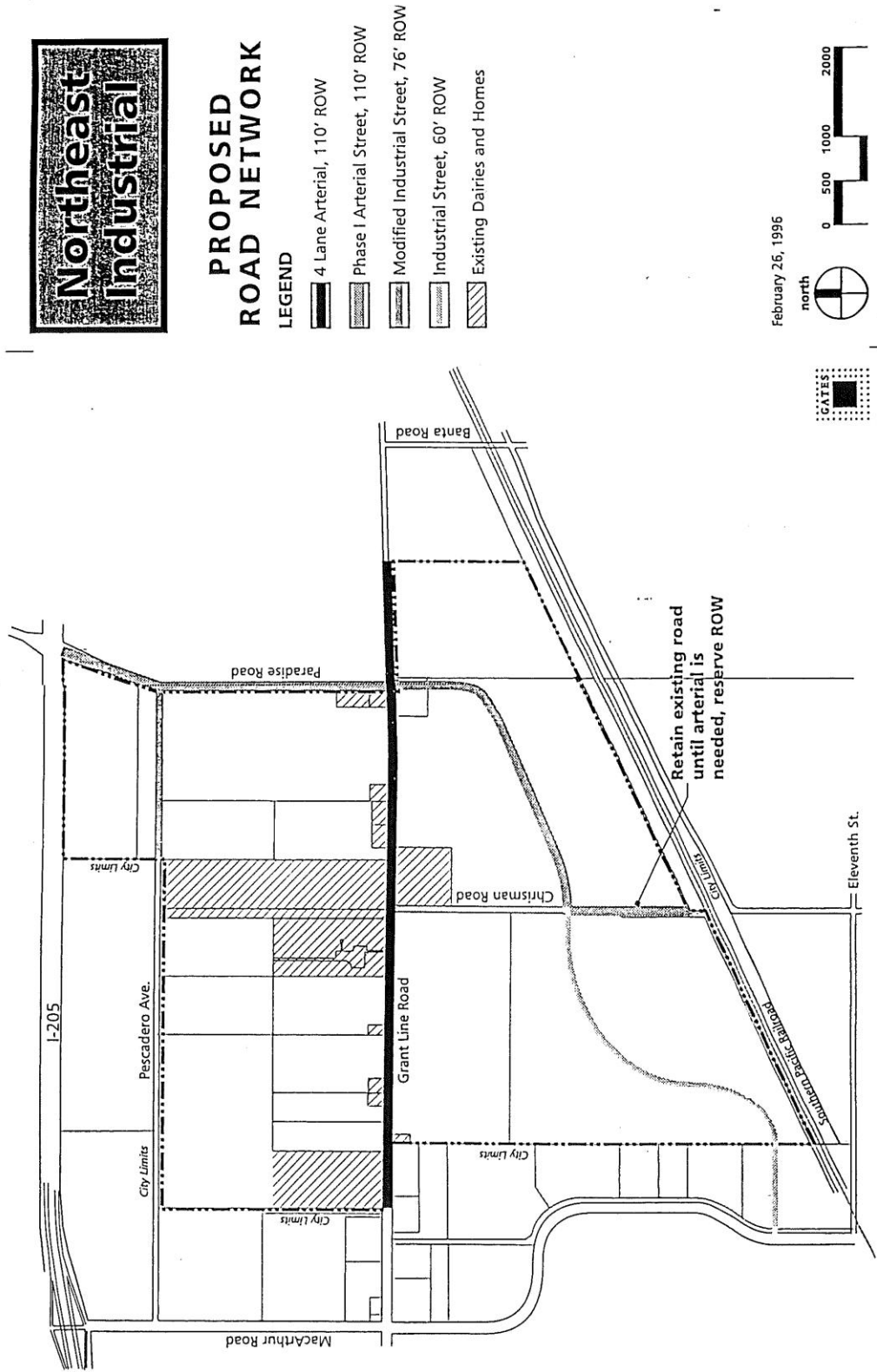
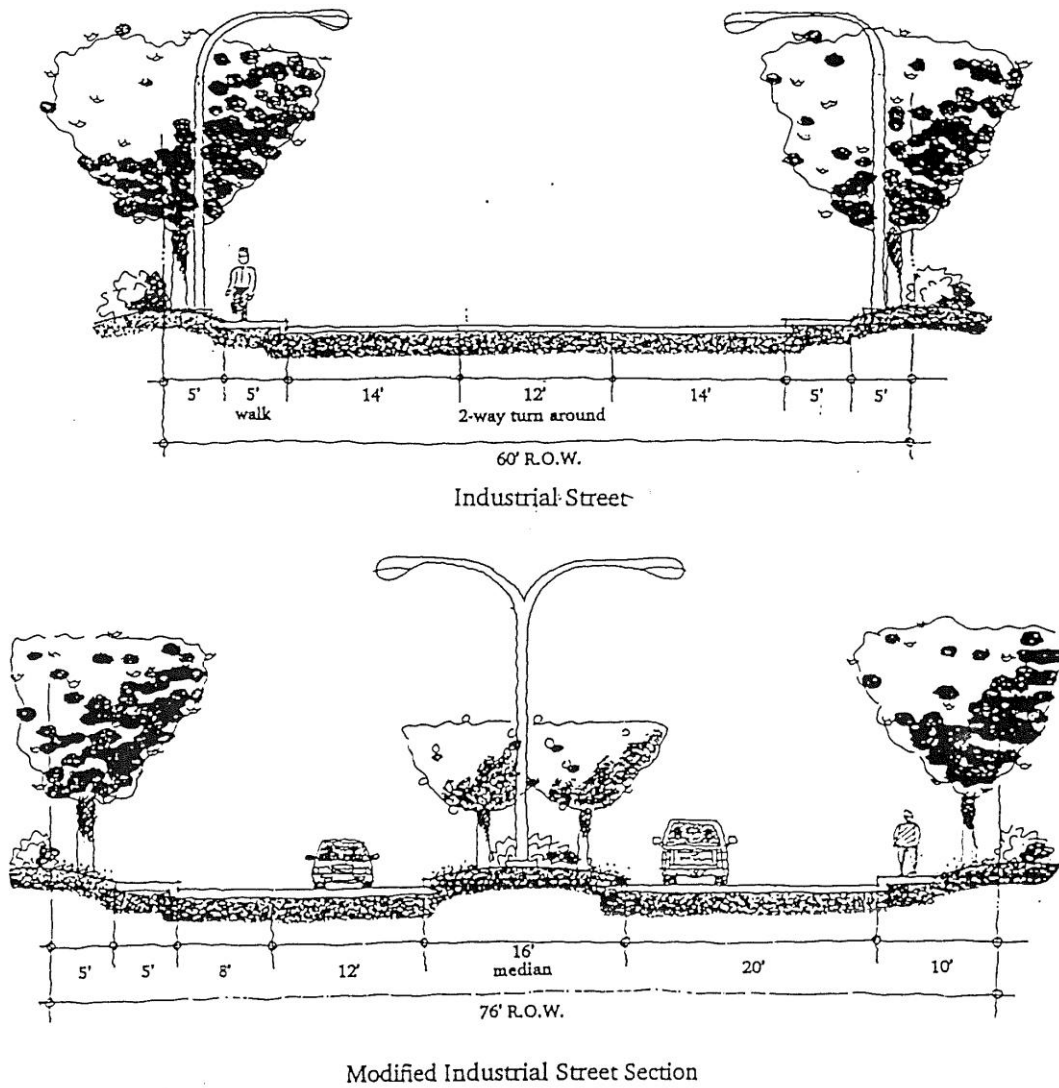


Figure 6



**STREET CROSS SECTIONS**

**Northeast Industrial**



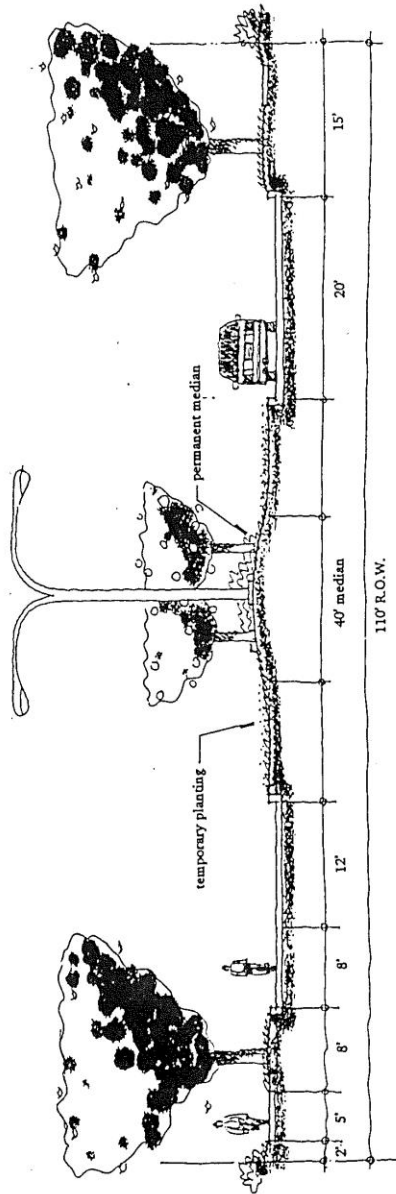
February 26, 1996



Figure 7A

# Northeast Industrial

## STREET SECTIONS



Phase 1 Industrial Street Section - preserves potential for expansion

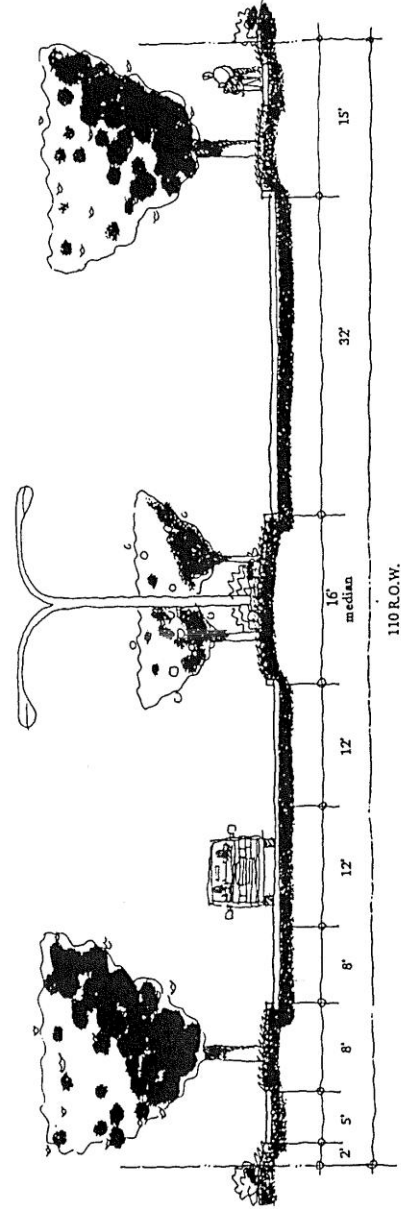


Figure 7B

February 26, 1996



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## **PUBLIC SERVICES AND INFRASTRUCTURE**

### **Wastewater Collection and Disposal**

The distribution, location and extent of the wastewater conveyance treatment and discharge within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future wastewater improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure 8 shows the master sewer plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

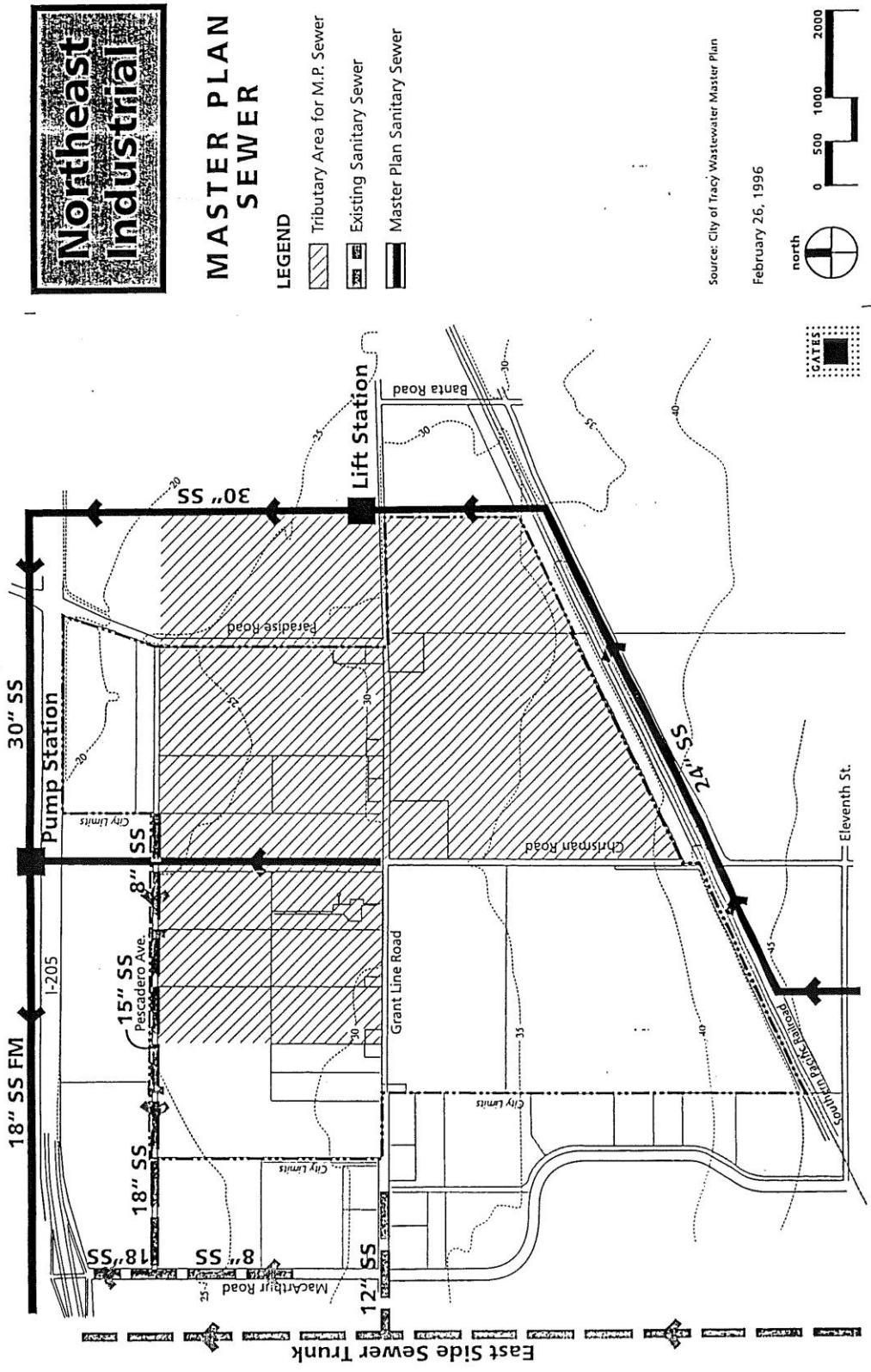


Figure 8

**Storm Drainage**

The distribution, location and extent of the storm drainage improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future storm drainage improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure 9 shows the original storm drainage master plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.



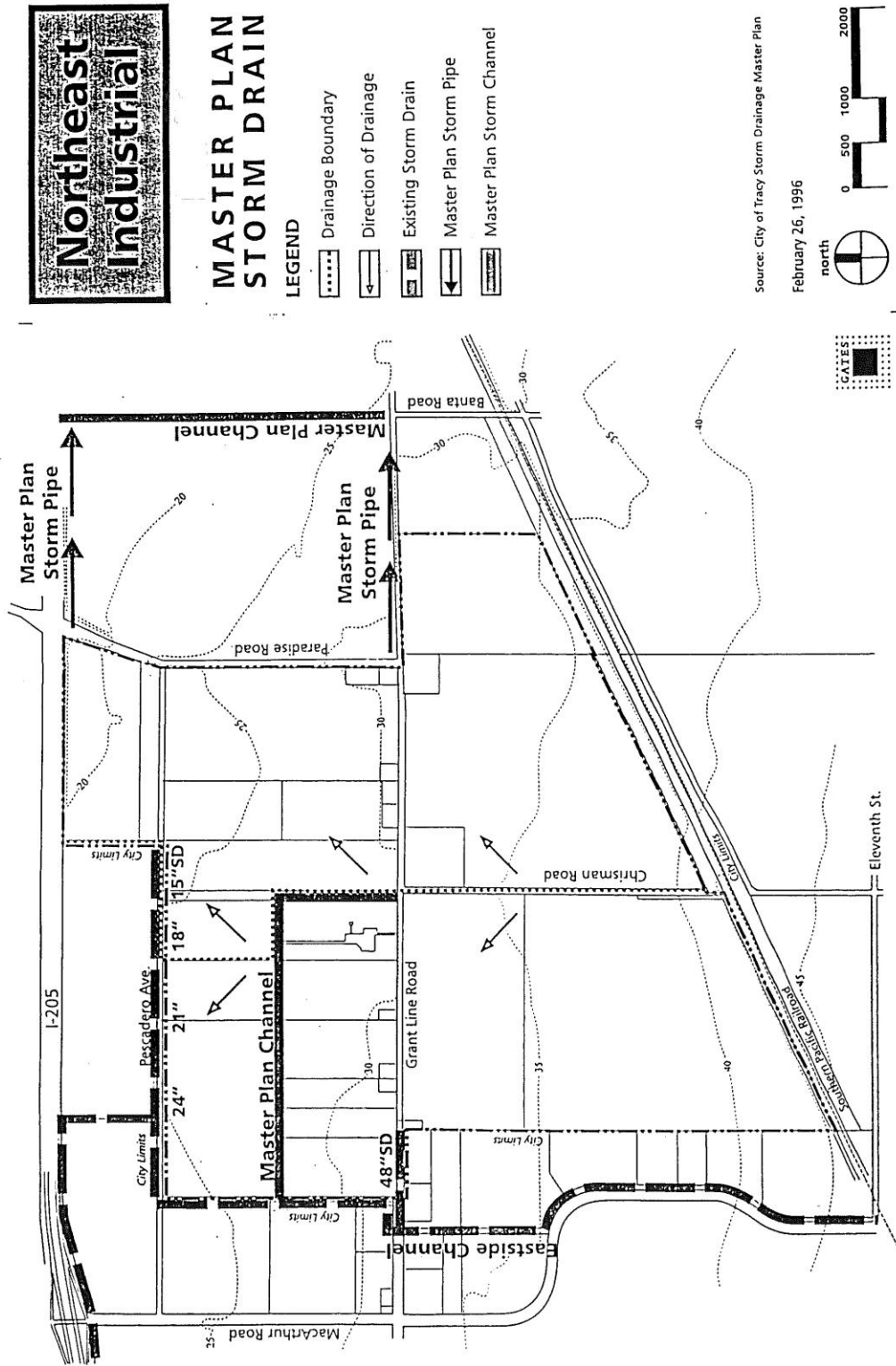


Figure 9

**Water Supply and Distribution**

The distribution, location and extent of the water improvements within the Specific Plan area shall be subject to the NEI Phase I Finance and Implementation Plans, dated December 1999 (Resolution Numbers 99-462 and 99-485), April 1, 2003 (Resolution Number 2003-100), January 4, 2005 (Resolution Number 2005-023), February 21, 2006 (Resolution Number 2006-069), and April 15, 2008 (Resolution Number 2008-065), and the NEI Phase II Finance and Implementation Plans, dated January 2006 (Resolution Number 2006-038) and January 15, 2008 (Resolution Number 2008-010). All future water improvements will also be subject to any revisions or updates to the NEI Finance and Implementation Plans, and subject to the development impact fees as established in those plans. Figure 10 shows the original water master plan for the Specific Plan, which will be modified by the Finance and Implementation Plan process.

**Police Protection**

Police services to the project will be provided by the City of Tracy Police Department.

**Fire Protection**

The planning area is located within the boundaries of the Tracy Fire Department. The Tracy Fire Department will provide fire protection service and paramedic ambulance service to the planning area.

**Other Utilities**

Pacific Gas and Electric (PG&E) will provide electricity and natural gas to the planning area. Pacific Bell will provide telephone service to the planning area.

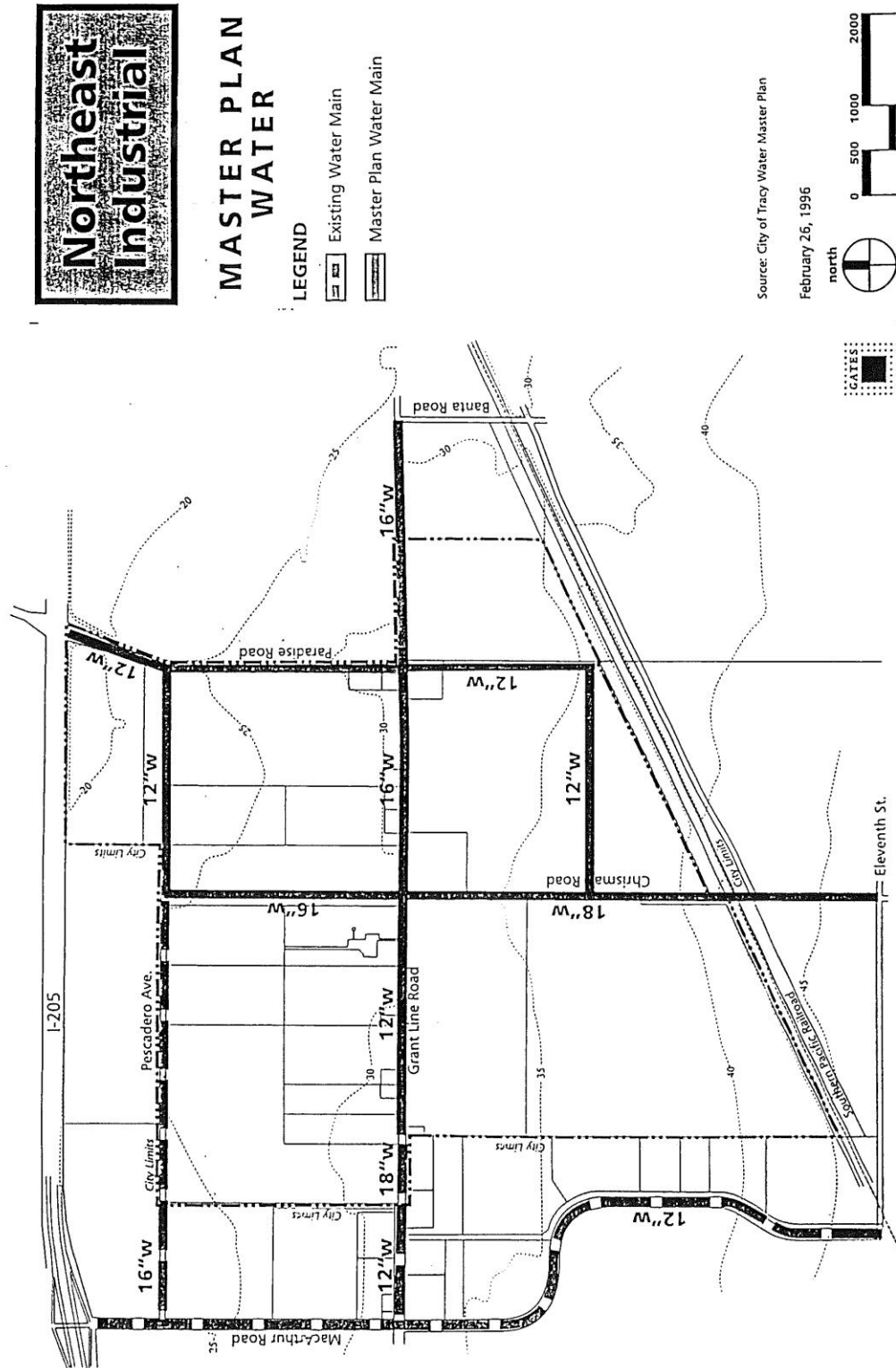


Figure 10

## DESIGN GUIDELINES

The Specific Plan includes design guidelines and development standards to guide site planning and architecture. These design guidelines are similar to those adopted in the Industrial Areas and I-205 Corridor Specific Plans, which are adjacent to the planning area.

### Streetscapes

1. The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, berming, lighting, and signage to produce an attractive and functional environment.
2. All landscaping should employ a mix of trees, shrubs, groundcovers and turf, where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs, and turf is encouraged, and compliance with the state’s water efficient landscape guidelines is required.
3. The use of lawn substitutes is encouraged in all medians and for parkways. The use of turf should be minimized and reserved for areas of high use or visibility and temporary median planting in anticipation of future street widths.
4. Automatic irrigation is required for all landscape areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, and parking areas, etc. The use of water conserving systems, such as drip irrigation for shrub and tree planting, is encouraged.
5. Tree plantings should reflect street hierarchy with larger trees along arterial streets and smaller trees on industrial streets. Tree plantings shall be symmetrical and of the same species in the parkways on both sides of the streets. One tree species or mixture of species shall be planted consistently at regular intervals along the entire length of a street. Spacing interval shall be no greater than 40 feet on center. Where trees are planted in medians, the plantings shall be continuous and at regular intervals. Spacing of median trees shall be no greater than 30 feet on center. Different tree species shall be planted at intersections to highlight these areas.
6. Adequate sight lines shall be maintained at all times

### Recommended Trees for Major Streets

The following list identifies recommended trees for the major streets that form the framework of the area:

| <i>Street</i>        | <i>Parkway</i>                | <i>Median</i>                 |
|----------------------|-------------------------------|-------------------------------|
| Grant Line Rd        | Pistacia chinensis            | Prunus serrulata cvs.         |
|                      | Chinese Pistache              | Flowering Cherry              |
| Paradise Rd/ Loop Rd | Celtis sinensis               | Pyrus calleryana 'Bradford'   |
|                      | Chinese Hackberry             | Bradford Pear                 |
| Pescadero Ave        | Fraxinus oxycarpa 'Raywoodii' | Pyrus calleryana 'Aristocrat' |
|                      | Raywood Ash                   | Aristocrat Pear               |

Select one of the following street trees for use on each industrial road:

| <i>Botanical Name</i>         | <i>Common Name</i> |
|-------------------------------|--------------------|
| Eucalyptus gunnii             | Cider Gum          |
| Fraxinus oxycarpa 'Raywoodii' | Raywood Ash        |
| Fraxinus uhdei                | Evergreen Ash      |
| Platanus acerifolia           | Sycamore           |

**Street Lighting**

1. Illumination standards for arterial and industrial streets should reflect the different right-of-way widths and functions.
2. Light fixtures and standards shall meet all safety standards and shall be employed throughout the length of the street. It is recommended that one lighting fixture style be employed for use on all streets. Where possible, light standards shall be located in medians.

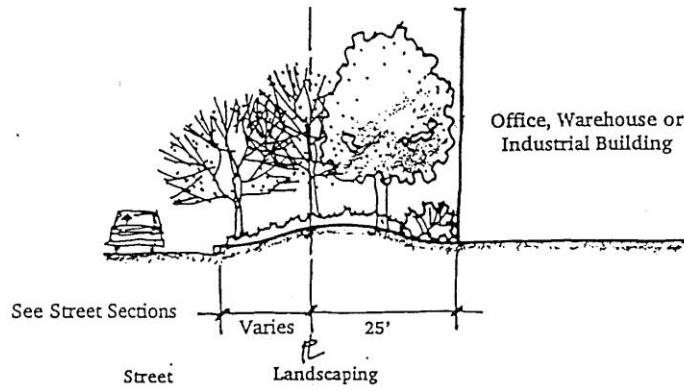
**Building Floor Area Ratio and Height**

| <i>Land Use</i>    | <i>Max. Floor Area Ratio</i> | <i>Max. Building Height</i> |
|--------------------|------------------------------|-----------------------------|
| General Commercial | 0.35                         | 46 ft.                      |
| Office             | 0.35                         | 60 ft.                      |
| Light Industrial   | 0.50                         | 60 ft.                      |

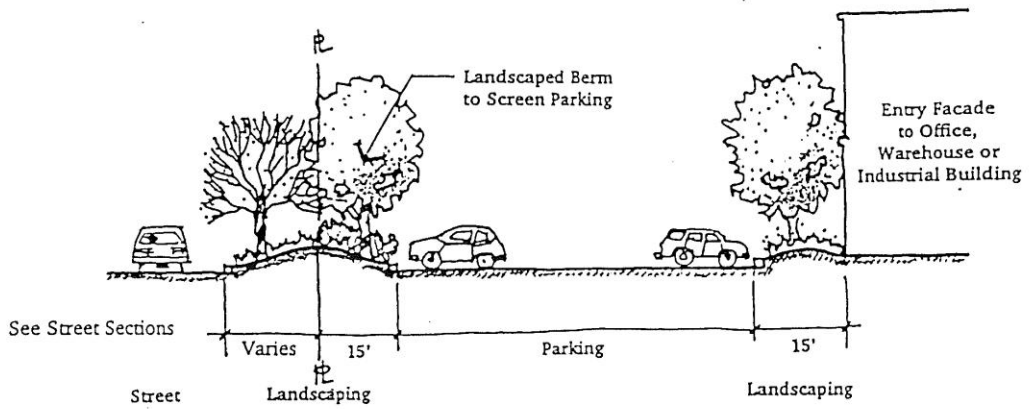
**Building Setbacks**

The following shall be the minimum building and parking setbacks required for all building types. Figure 14 illustrates these guidelines.

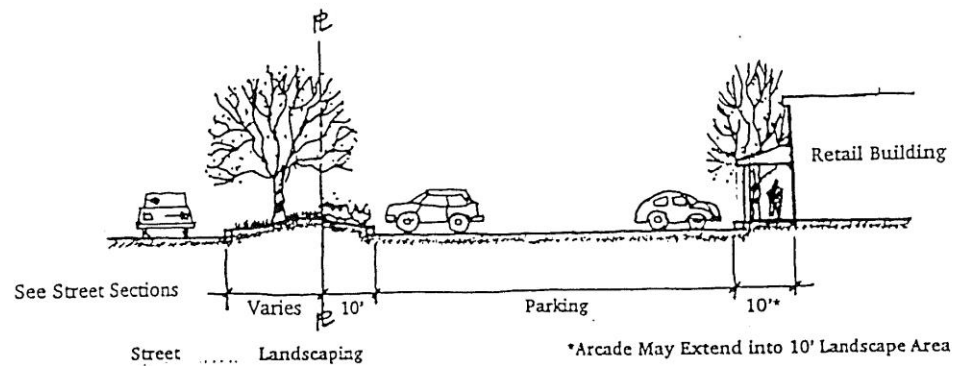
1. Building setback from any property line adjacent to a street or Caltrans right-of-way shall be 25 feet minimum. Rear and side yard building setbacks from property lines not adjacent to a street or Caltrans right-of-way shall be 15 feet minimum.
2. A 5 foot wide landscape setback is required along property lines not adjacent to a right-of-way. On the property lines perpendicular to the street frontage on industrial streets, the landscaped setback is only required to a point 150 feet onto the parcel from the street right-of-way or 50 feet back of building face, whichever is greater.
3. Parking setback from any property line along a public street or the Caltrans right-of-way for commercial land uses shall be 10 feet and for industrial uses shall be 15 feet.
4. Parking shall not be permitted within 10 feet of the building entry face of any commercial structure. In the event the building has an arcade or other shade structure along this frontage, the structure can be located within this required setback. Parking shall not be permitted within 15 feet of the office face or portion of a building. On industrial buildings, a 15 foot setback to the parking area shall be provided at building entries.
5. Commercial buildings shall be sited so as to create and enhance the streetscape. This can only be accomplished if all or a portion of the buildings are located near the street. On commercial sites of over 4 acres, at least one building must be located with a minimum setback from public-right-of-way to building face of 50 feet. On corner sites adherence to this requirement is encouraged on both frontages, however, only required on the major street frontage.



Front Yard Setback: Without Parking



Front Yard Setback: With Parking



Front Yard Setback: With Parking

### SETBACK GUIDELINES



February 26, 1996



Figure 11

**Parking and On-Site Vehicular Circulation**

1. Parking, on-site circulation, and loading area standards shall be as required by the provisions of Title 10, Article 26, Off-Street Parking Requirements of the Tracy Municipal Code unless modified below or as part of the Development Review approval. Portions of off-street parking requirements are summarize below.
2. Parking lots containing 10-20 spaces may include a maximum of 20% of the total number of spaces for compact cars. These spaces shall be designed and marked in accordance with City standards and distributed throughout the lot. Parking areas containing 20 or more spaces may include a maximum of 30% of the total number of spaces for compact cars.
3. Minimum off-street parking standards:

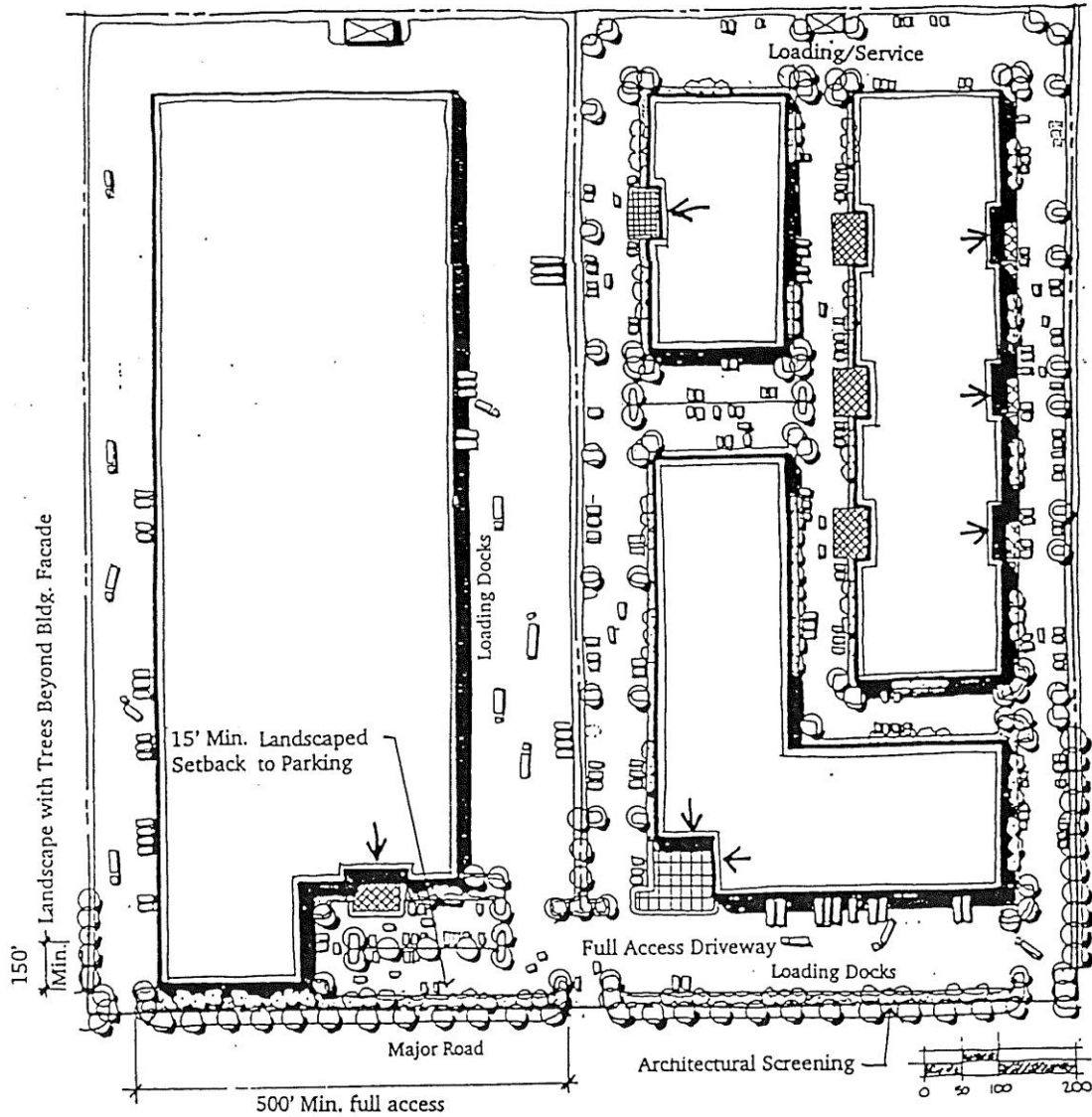
| <b><i>Uses</i></b>  | <b><i>Minimum Parking Spaces Required</i></b>  |
|---|--|
| Retail  | One space per 250 square feet of gross floor area  |
| Vehicle sales and rentals, including recreational vehicles and mobile homes                     | One space per 250 square feet of gross floor area plus one space per vehicle for sale or stored on lot.  |
| Office: business, professional (not including medical or dental), banks                         | One space per 250 square feet of gross floor area  |
| Dental and medical clinics or office  | One space per 200 square feet of gross floor area  |
| Motor vehicle repair garages  | One space per 600 square feet of gross floor area; repair stalls not counted as parking spaces   |
| Cafes, restaurants, and other establishments for the sale and consumption of food and beverages | Dining: one space per 45 square feet of customer area and one space per 250 square feet of all other area, plus additional spaces connected to uses such as drinking establishments. Drinking bars, cocktail lounge: one space per 35 square feet of drinking, bar, lounge area. |
| Manufacturing   | One space per 600 square feet of gross floor area, or if the number of employees on the maximum work shift can be verified, one space per one employee on the maximum work shift   |
| Warehouses/Storage and Distribution   | One space per 1,000 square feet of the first 20,000 square feet of gross floor area, plus one space per 2,000 square feet of the second 20,000 square feet of gross floor area, plus one space per 4,000 square feet of the remaining square feet of gross floor area.           |

**Loading and Unloading Spaces**

1. Sufficient off-street loading and unloading spaces shall be provided on each site, and adequate provisions and space shall be made for maneuvering freight vehicles and handling all freight. All loading activity, including turnaround and maneuvering, shall be made on site.
2. In commercial areas, truck loading areas and docks shall not be permitted between building(s) and the public street unless enclosed with architectural screen of material similar to building.

3. In industrial areas, truck loading areas and docks shall not be permitted between building(s) and the street unless the building(s) are set back from the curb a minimum of 125 feet and doors are screened by landscaping, berms, and/or fences.
4. Buildings, structures, and loading facilities shall be designed and placed upon the site so that vehicles, whether rear loading or side loading, may be loaded or unloaded at any loading dock, door, or area without extending beyond the property line.





Warehouse / Storage / Distribution  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 213,000 s.f.  
 73 Parking Stalls

Light Manufacturing / R&D  
 Example:  
 10 acres (gross)  
 FAR 0.50 max.  
 193,000 s.f.  
 429 Parking Stalls

**LIGHT MANUFACTURING / R&D PROTOTYPE**



February 26, 1996



Figure 12

## Driveway Standards

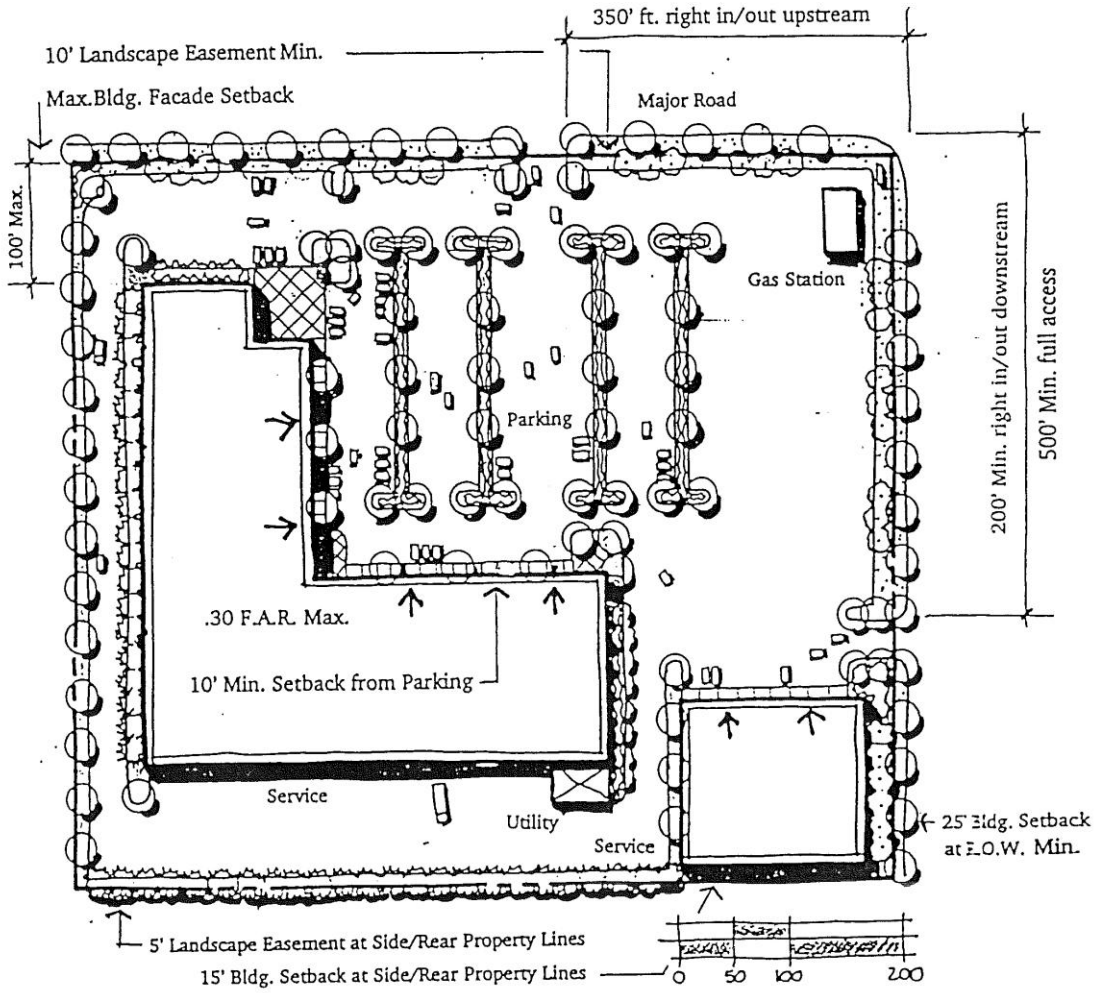
Driveways should be carefully located so as not to impede the primary function of the streets, which is to carry through traffic. It should be noted that these spacing guidelines are minimum values. The goal should be to exceed them where possible.

1. Individual industrial parcels on major arterial streets may have driveways, but they should be carefully located so as not to impede the traffic efficiency. In general, parcels with frontage on the major arterials should have their entryway on side streets if possible. If a parcel's only frontage is on the major arterial, every effort should be made to consolidate access at a single driveway. Spacing standards for driveways on major arterials shall be as follows:
  - a. Full access driveways , 500 ft. minimum
  - b. Partial access driveways (right in/out, left turn in), 500 ft. minimum
  - c. Right turn in and out, 350 ft. minimum upstream from an intersection
  - d. Right turn in and out, 200 ft. minimum downstream from an intersection
2. On industrial streets, spacing for full access driveways is 450 feet, minimum. "T" intersections are encouraged over four-way intersections. Every effort should be made to consolidate driveways.
3. No driveway shall be located closer than 200 feet to the radius return point at intersections.
4. Driveways shall be a minimum of 25 feet wide. Subsequent development shall demonstrate driveway width and placement can accommodate truck turning movement and clearing without blocking roadways.
5. Driveway width modifications may be approved with shared (ganged) driveways. Ganged driveways which serve two adjacent sites will be required to install landscaped islands along parking adjacent to the gang driveway and a landscape zone at the end of the common drive will act as a terminus to the view line down the ganged driveway.
  - a. Full curb returns (as opposed to a standard driveway) shall be utilized for entries to all sites of over ten acres in size or for common driveways that serve two adjacent sites that together total more than ten acres.
6. Access driveways shall provide adequate length to accommodate off-street vehicle stacking needs during times of peak use.
7. Parcel entry should be clear, attractive, and inviting; circulation should direct employee and visitor traffic clearly through the site to main building entries and drop-off points and service trucks to loading.
8. In commercial areas, vehicular entries to the site shall be well defined and recognizable to motorists. Improvements should include accent paving, signs, special plantings, and lighting. Such improvements shall not block motorists' sight lines to oncoming traffic.

## Freeway Interface

The control of views of Tracy from I-205 is critical for the establishment of a quality image for the community.

1. Locate services and storage areas to minimize visibility from I-205.
2. All freeway setback zones shall be planted with a combination of trees, shrubs, and groundcover. Automatic irrigation is required of all planted area. Use large scale trees, from a 24-inch box minimum, grouped in single species clusters. Mass trees to avoid blocking views of commercial signage while providing at least one tree per 1,500 square feet of setback area. Plant shrubs in an informal hedge near the property line with gaps between hedges of 50 feet maximum. Install from 5 gallon cans, minimum, in single species clusters at least 100 feet long. Hydroseed or otherwise install permanent groundcover in all places not planted with shrubs.



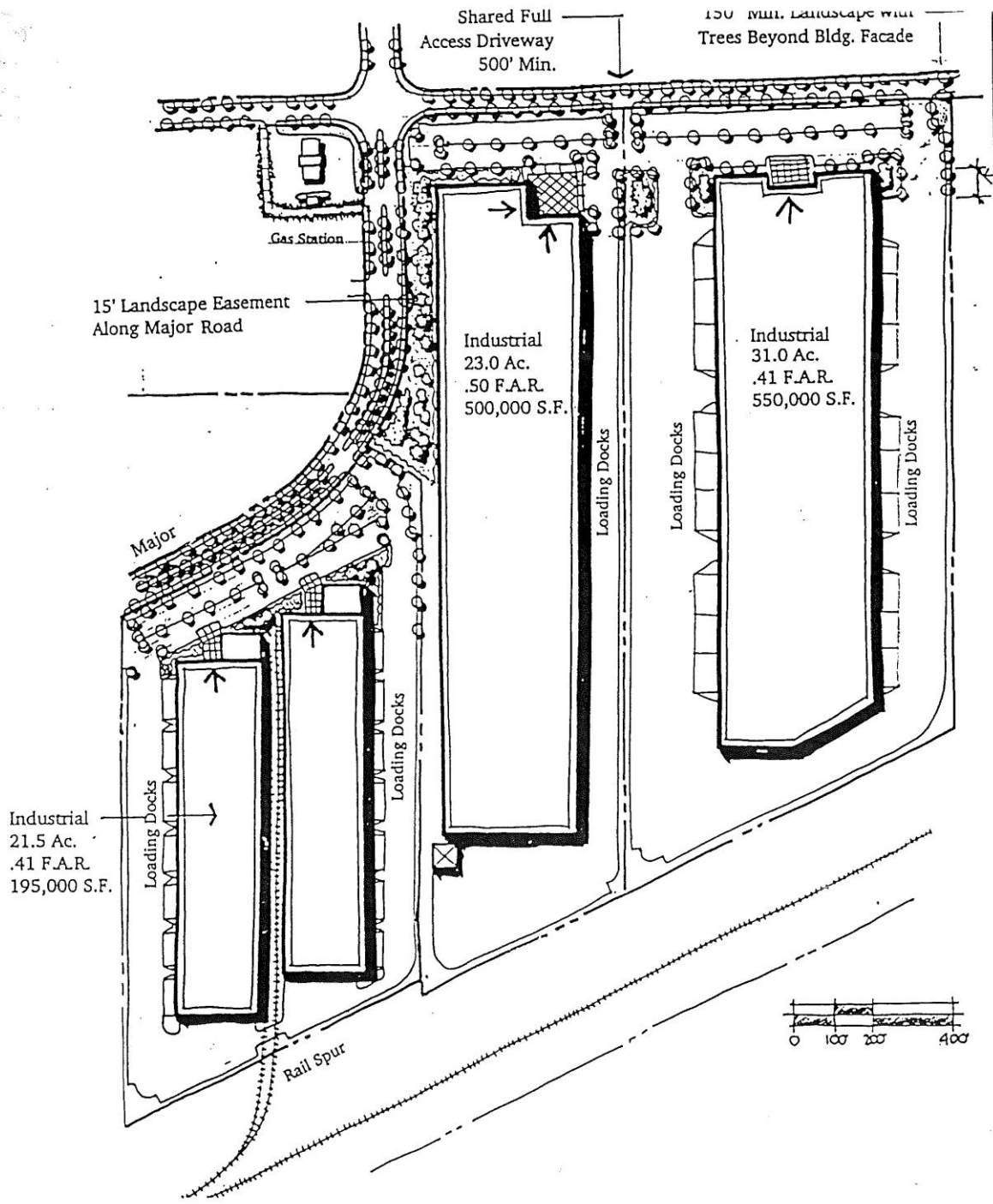
COMMERCIAL PROTOTYPE



February 26, 1996



Figure 13



### WAREHOUSE PROTOTYPE



February 26, 1996



Figure 14

## Building Architecture

1. Use of creative building design and construction techniques is encouraged. Special attention should be given to that portion of the building visible from adjacent roadways or public parking areas.
2. Large buildings should have facades that include variations in massing, form, and texture. Continuous surface treatments of a single material should be minimized. Architecture should be used to highlight building entries.
3. Any accessory buildings and enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.
4. Continuous arcades along the front of commercial buildings are encouraged as they provide the pedestrian protection from the weather, reduce solar gain, and can serve to enhance the character of what might otherwise be simple, formless structures. When more than one building is to be developed on a commercial site, the buildings should be designed to relate to one another as a total composition with well thought out relationships to one another.

## Signs

1. Signs must conform to the requirements of Signs, Title 10, Article 35 of the Tracy Municipal Code as modified herein.
2. A site sign program should be integrated into a total design concept for a site and its buildings. The primary goal of the project sign system is to provide information and identification. When more than one sign is permitted, all signs shall be of similar style, shape, and materials.
3. All signs must be approved prior to installation, and should be designed in a manner that coordinates the sign designs and locations with the site plan and building architecture for each project. The sign plans should include:
  - a. **Detached Business Identification Signs:** One such monument sign (as defined by the Tracy Municipal Code) shall be allowed for each street frontage of the site. These signs may only contain the symbol and/or name of the business and its street address. The sign shall be free standing, may be double-sided, and shall be set back a minimum of 5 feet from the public right-of-way. Sign area shall not exceed 32 square feet per frontage and sign shall not exceed 6 feet in height from finished grade. Signs should generally be oriented perpendicular to approaching traffic.
  - b. **Wall signs:** On large single tenant buildings, signs should be located immediately above or adjacent to the primary building entrance. No sign shall extend above dominant roof lines. The area of any single sign shall not exceed 100 square feet. Total area shall not exceed one-half square foot of sign per lineal foot of business being served.

On smaller multi-tenant buildings, signs should be located at the frontage of each individual lessee. The area of any single sign shall not exceed 100 square feet nor more than 75 percent of the tenant frontage. Capital letters shall be no more than 2.5 feet in height and lower case letters no more than 1.5 feet in height. When individually-lettered wall signs comprise over 50 percent of the sign area of all sign types, total sign area shall not exceed 1.2 square feet per lineal foot of business being served. When comprising less than 50 percent of the total sign area, the maximum sign area shall be one-half square foot per lineal foot of business being served.

- c. **Directional Signs:** Signs required or desired to assist patrons in accessing the facility shall be located in the site parking areas. The design of such signs shall be simple and easily legible.

There is no limit to the number of signs provided on a site; however, no single sign shall exceed 6 square feet in area, except that vehicular “stop” signs shall be mounted per State standards.

4. A sign may be illuminated provided that no flashing, traveling, animated, or intermittent illumination shall be used. Such illumination shall be confined to the area of the sign except when such illumination is back lighting for an otherwise non-illuminated sign. No sign illumination shall cast a glare which is visible from any street.
5. Signs should be constructed with quality materials and in a craftsman-like manner to ensure both an attractive appearance and durability.

**Landscaping**

Minimum on-site landscaping requirements shall be established by Off-Street Parking Requirements (Title 10, Article 26 of the Tracy Municipal Code), except as modified below.

| <i>Summary of Requirements</i>              | <i>Commercial</i>   | <i>Industrial</i>    |
|---|---------------------|----------------------|
| Landscaped frontage setback                 | 10 feet             | 15 feet              |
| Minimum number of trees in parking area     | 1 tree per 5 spaces | 1 tree per 10 spaces |
| Percentage of landscaping in parking areas: |                     |                      |
| 0 - 15 cars                                 | 5%                  | 5%                   |
| 16 - 30 cars                                | 10%                 | 5%                   |
| 31-60 cars                                  | 15%                 | 7½%                  |
| Over 60 cars                                | 20%                 | 10%                  |

1. While commercial uses benefit from a well-landscaped parking area and visibility from the street, views of industrial uses benefit from a more generously landscaped streetscape. Thus, parking lot landscaping requirements for industrial uses may be reduced as specified in the Off-Street Parking Requirements in order to create a large landscape setback along the street. These provisions allow the reduction of 50 percent of the required landscaping based on the provision of a 15 foot landscape setback along the street frontage. The 15 foot strip may be included in the calculation of the total parking lot landscaping requirement. The remainder of the landscaping requirement must be distributed over the lot(s) to provide shade and landscape building frontage. Canopy trees shall be evenly distributed throughout the parking area to provide shade.
2. On-site landscaping along rights-of-way between property lines and buildings, parking lots, or vehicular circulation improvements shall be installed by the property owner. This landscaping shall be designed as an extension of the adjacent public right-of-way landscaping. Completion of landscaping on the site shall be simultaneous with completion of the building and other improvements on the site.
3. Landscaping shall not obstruct sight lines at street or driveway intersections.
4. In place of the wheel stops at parking lots, landscape areas and pedestrian walkways may be extended not more than 2 feet into required parking spaces, to include a 6” concrete curb. In such cases, no credit toward parking lot landscape requirements shall be given for the resulting additional landscaping.
5. Screening of the parking area from public rights-of-way in industrial areas shall be provided with a 2½ to 3 foot high element, measuring from the top of the parking area pavement. Screening may consist of one or a combination of the following:
  - a. Berms landscaped with ground cover, trees, and shrubs;
  - b. Solid, low profile, decorative masonry walls;
  - c. Evergreen shrubbery which, when solely used as screening, shall be continuously maintained to provide solid screening.

6. Generous landscaping screening is required adjacent on all street frontages for industrial areas. These areas should be landscaped with a combination of trees, shrubs, and ground cover to soften views of parking areas.
7. Tree planting and selection and massing should be compatible with streetscape plantings. Provide minimum 1 tree per 400 square feet of landscape setback. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together.
8. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs, and turf is encouraged. The use of turf in the narrow planting islands is discouraged.
9. Live plant materials shall be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole groundcover material.
10. All trees shall be of 24 inch box size minimum at planting with a minimum branching height 5 years after installation of 10 feet above road or parking surfaces and 6 feet at pedestrian areas. Shrubs shall be of 5 gallon size minimum with a maximum on-center spacing of 24 inches. Likewise, groundcover may be planted at 1 gallon size minimum with a maximum spacing of 12 inches on center.
11. Automatic irrigation is required for all landscaped areas. Irrigation systems shall be designed so as not to overspray walks, buildings, and parking areas.

### **Screening and Storage**

1. All exterior trash areas, storage structures, and service areas shall be screened from public view with a wall or fence of a minimum height of 8 feet above the street curb level. Storage areas shall be set back a minimum of 50 feet from streets, unless fully enclosed in an architecturally compatible enclosure.
2. No storage areas are allowed within the landscape easements, front setbacks, or side or rear yard landscaped buffers.
3. Roof-mounted equipment shall be screened from street view. Pad-mounted transformers, utility connections, and meter boxes shall be screened and integrated into the site plan.
4. The design of masonry walls, fencing, trash enclosures, and similar accessory site elements should be compatible with the architecture of the building and should use similar materials. Where masonry walls are along property frontage, they should enhance the entrance to the property and should not impair traffic safety by obscuring views. Long expanses of wall surfaces should be architecturally designed to prevent monotony.
5. The use of chain link fences shall be discouraged, and no chain link fences shall be visible from any public right-of-way.

### **Development Review Process**

All development in the NEI shall be subject to the requirements of Tracy Municipal Code Article 30, Development Review (TMC Sections 10.08.3920 through 10.08.4110).

### **Environmental Performance Standards**

#### **Use Restrictions**

No use shall be permitted to exist or operate on any lot which:

1. Emits dust, sweepings, dirt, cinders, fumes, odors, radiation, gases and vapors, or discharges liquid or solid wastes or other harmful matter into the atmosphere or any body of water which may, according to the appropriate agency, adversely affect the health and safety persons within the area or the health and safety of persons in adjacent areas or the use of adjacent properties.

2. Discharges waste or any harmful substance, as defined by the Municipal Code, into any public sewer or storm drainage system.
3. Produces intense glare or heat, unless such use is performed only within an enclosed or screened area, and then only in such manner that glare or heat emitted will not be discernible from any exterior lot line.
4. Creates a sound pressure level in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
5. Allows the visible emissions of smoke (outside any building) other than the exhausts emitted by motor vehicles or other transportation facilities or any emissions in violation of any regulation of any public body having jurisdiction. This requirement shall also be applicable to the disposal of trash and waste materials.
6. Creates a ground vibration that is perceptible, without instruments, at any point along any of the exterior lot lines.

### **Hazardous Wastes and Water Pollutants**

1. All new industries locating with the area will be required to obtain a Discharge Permit from the Director of Utilities prior to occupancy. This permit shall establish the amount and quality of wastes allowed to be discharged into the City's sanitary sewer.
2. The quality of wastewater entering the city sewage system from the proposed uses shall be measured by the Biochemical Oxygen Demand (BOD) and Total Suspended Solids (TSS) levels referenced in the local Water Quality Control Board 208 Plan. Users that are not expected to comply with these standards will be required to provide on-site pretreatment facilities.
3. The storage and distribution of hazardous materials shall be subject to the rules of the San Joaquin County Health District.
4. Industries regularly using significant quantities of hazardous chemicals as defined by State Law in the course of their operations shall be required to obtain a Conditional Use Permit.

### **Mineral Extraction**

1. Mineral extraction shall only be permitted as a subsidiary use to main use of the site.
2. Locate equipment, storage, and facilities for mineral extraction to avoid visibility from the public street.

### **Dairy Uses**

1. With development applications for an individual site, provide information demonstrating provisions of adequate buffers between proposed development and adjacent existing dairy uses.
2. Existing dairy uses are exempted from the Environmental Performance Standards.

### **Permitted and Conditionally Permitted Uses**

The following table indicates uses which are to be permitted and permitted subject to Conditional Use Permits in the Northeast Industrial area. The land use designations are abbreviated as follows:

LI: Light Industrial

GC: General Commercial



The uses shown in Table 18 as Permitted (“P”) are those that are deemed acceptable anywhere in the assigned land use designation. They are uses that, when developed in conformance with this Plan, will not require special conditions in order to avoid negative impacts.

The uses shown as Conditionally Permitted (“C”) are of two types. Some of these uses are not acceptable in every location within a land use but are acceptable in certain locations. Other uses may require special conditions to make them acceptable at particular locations, due to their potential negative impacts on existing or planned uses. This may be because of their potential nuisance aspects, such as noise or hazardous wastes.

Where neither a “P” or “C” is shown for a particular land use district, that use is not allowed.

*Table 1: Permitted and Conditionally Permitted Land Uses*

| <b>Land Uses</b>  | <b>LI ●</b> | <b>GC</b> |
|---|-------------|-----------|
| Agricultural, including dairies   | P ◊         | P ●, ◊    |
| Accessory uses and structures; not including warehouses located on the same site as a permitted use   | P           | P         |
| Administrative, executive, research, medical offices  | P           | P         |
| Call centers  | P           | P         |
| Accessory uses and structures located on the same site as a conditional use   | C           | C         |
| Warehousing and distribution facilities   | P           |           |
| Manufacturing, repair, assembly, or packaging of products from previously prepared materials, such as cloth, plastic, leather, or semi-precious metals or stones, but not including such operations as saw or planing mills, any manufacturing involving primary production of wood, metal, or chemical products from raw materials | P           |           |
| Manufacture of food products, pharmaceuticals, biotechnology products and the like, but not including fish or meat products, sauerkraut, vinegar, or the like, or rendering or refining of fats and oils.   | P           |           |
| Laboratories, including chemical, physical materials testing, electronic, agricultural, photographic film processing, and general research  | P           |           |
| Electrical industrial apparatus manufacturing, service, and repairs, including motors, generators, welding equipment, electrical transmission and distribution equipment, and turbines and pumps.   | P           |           |
| Manufacture, repair of optical electronic, timing, and measuring instruments  | P           |           |
| Dairy products plants   | P           |           |
| Machine shops   | P           |           |
| Heating, plumbing, and ventilating equipment manufacturing, servicing, repairs  | P           |           |
| Refrigerator, furnace, water heater, and other household appliance manufacturing, service and repairs, not incidental to retail sales   | P           |           |

|   |   |   |
|---|---|---|
| Furniture and cabinet assembling whose activities are carried on entirely within an enclosed building and which have no construction yards on the lot   | P |   |
| Parcel delivery service and vehicle storage inside and outside the building   | P |   |
| Truck terminals   | P |   |
| Mini storage  | P |   |
| Equipment storage   | P |   |
| Janitorial services and supplies  | P |   |
| Printing, including lithographing, engraving, and other such similar reproduction services  | P |   |
| Automotive supply stores  | C |   |
| Rental yards, including the rental of hand tools, garden tools, power tools, trucks, trailers, and other similar equipment  | C |   |
| Building materials sales, lumberyards (outside storage)   | C |   |
| Repair, painting, and body work for automotive, motorcycle, and farm machinery  | C |   |
| Boat sales, service, repair   | C |   |
| Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, and retail products typically found in a convenience store. | C |   |
| Wholesale trade business  | C |   |
| Intermediate manufacturing uses involving the processing of raw materials, including food and paper processing, wineries, and concrete mixers   | C |   |
| Mineral and hydrocarbon extraction  | C |   |
| Recycling (collection and sorting)  | C |   |
| Outlet stores and centers   |   | P |
| Warehouse retail, including furniture, office supply, sporting goods, or wholesale merchandising  |   | P |
| Hardware stores, including garden centers   |   | P |
| Home improvement and interior decorating stores, including carpet, drapery, floor covering, paint, glass, and wallpaper shops   |   | P |
| Garden centers, including plant nurseries and retail sales  |   | P |
| Sporting goods and toy stores   |   | P |
| Specialty retail stores selling those items and services normally sold in department stores (including clothing, shoes, and accessories)  |   | P |
| Shoe and clothing repair  |   | P |
| Food markets, convenience markets   |   | P |
| Liquor stores   |   | P |
| Drug stores and prescription pharmacies   |   | P |
| Restaurants, including fast food  |   | P |

|   |  |   |
|---|--|---|
| Art galleries and artists supply stores   |  | P |
| Barbershops, beauty shops, and hairstylists   |  | P |
| Financial institutions, including banks, savings and loan offices, finance companies, credit unions, and related services   |  | P |
| Refrigerator, furnace, water heater, and other household appliance sales including repair, provided repair services are incidental to retail sales.   |  | P |
| Laundries and dry cleaners  |  | P |
| Photocopying and related duplicating services, not including printing, lithographing, engraving, or such similar reproduction services  |  | P |
| Pet and bird stores with incidental veterinary services   |  | P |
| Animal Shelters   |  | P |
| Video stores, sales, and rental   |  | P |
| Automobile and motorcycle sales and rental, including new and used sales  |  | C |
| Bars without significant food service   |  | C |
| Building materials sales (no outside storage)   |  | C |
| Miniature golf  |  | C |
| Service stations, provided all operations except sales of gas and oil are conducted within an enclosed building. Sales shall be limited to petroleum products and automotive accessories, and retail products typically found in a convenience store. |  | C |

●: Subject to compliance with the Environmental Performance Standards. Note that as defined by State law, any business using significant quantities of hazardous materials requires a Conditional Use Permit.

◇: Existing dairy operations are exempt from the Environmental Performance Standards.